



**BICYCLE/PEDESTRIAN ADVISORY
COMMITTEE**

AGENDA

NOTICE AND AGENDA

SPECIAL MEETING – WEDNESDAY, SEPTEMBER 17, 2014
PLAZA CONFERENCE ROOM AT CITY HALL – 500 CASTRO STREET
6:30 P.M.

1. **CALL TO ORDER**
2. **ROLL CALL**—Committee members Bruce England, Josette Langevine, Marc Roddin, Vice Chairperson Simon Purdon, and Chairperson Greg Unangst.
3. **ORAL COMMUNICATIONS FROM THE PUBLIC**

This portion of the meeting is reserved for persons wishing to address the Committee on any matter not on the agenda. Speakers are limited to three minutes. State law prohibits the Committee from acting on nonagenda items.

4. **MINUTES APPROVAL**

Minutes for the August 27, 2014 meeting have been delivered to Committee members and copies posted on the City Hall bulletin board. If there are no corrections or additions, a motion is in order to approve these minutes.

5. **UNFINISHED BUSINESS**—None.

6. **NEW BUSINESS**

6.1 MODIFICATIONS TO CASTRO STREET

Overview:

The Committee will receive a presentation regarding the draft conceptual plans for modifications to Castro Street, between El Camino Real and Miramonte Avenue.

Recommendation:

Provide input on the proposed modifications to Castro Street, as part of Project 14-36, Modifications to Castro Street, Between El Camino Real and Miramonte Avenue.

6.2 PERMANENTE CREEK TRAIL, CHARLESTON ROAD CROSSING

Overview:

The Committee will receive an overview of the Permanente Creek Trail crossing at Charleston Road and provide input on bicycle- and pedestrian-related issues and opportunities for the trail crossing.

Recommendation:

Provide input on the bicycle- and pedestrian-related issues and opportunities associated with Permanente Creek Trail crossing at Charleston Road.

6.3 MOBILITY COORDINATOR CANDIDATES – COMMUNITY INTERVIEW PANEL

Overview:

The Committee will consider appointing a Bicycle/Pedestrian Advisory Committee (B/PAC) representative and alternate (in case of scheduling conflict) to the community panel interviewing Mobility Coordinator candidates.

Recommendation:

Appoint a B/PAC representative and alternate to the community panel interviewing Mobility Coordinator candidates.

6.4 UPCOMING AND RECENT EVENTS

Overview:

The Committee will discuss the members' participation in the Council Neighborhoods Committee neighborhood meetings, Farmers' Market, Suggested Routes to School Program, and other events.

Recommendation:

None.

6.5 FISCAL YEAR 2014-15 BICYCLE/PEDESTRIAN ADVISORY COMMITTEE WORK PLAN UPDATE

Overview:

The Committee will review the Fiscal Year 2014-15 B/PAC Work Plan and provide updates on the Committee's recent activities.

Recommendation:

None.

6.6 VALLEY TRANSPORTATION AUTHORITY (VTA) BICYCLE & PEDESTRIAN ADVISORY COMMITTEE (BPAC) UPDATE

Overview:

The Committee will receive a report from the City's VTA BPAC representative on the VTA BPAC agenda items.

Recommendation:

Comment on the VTA BPAC agenda items.

7. COMMITTEE/STAFF COMMENTS, QUESTIONS, AND REPORTS

No action will be taken on any questions raised by the Committee at this time.

7.1 STAFF COMMENTS

7.2 COMMITTEE COMMENTS

8. SET DATE AND TIME FOR NEXT MEETING

Wednesday, October 29, 2014, at 6:30 p.m.

9. **CALENDAR**

Wednesday, November 19, 2014 – B/PAC Special Meeting

Thursday, October 2, 2014 – Monta Loma/Farley/Rock Neighborhood Meeting
(Unangst)

10. **ADJOURNMENT**

HK/5/PWK
915-09-17-14A-E

AGENDAS FOR BOARDS, COMMISSIONS, AND COMMITTEES

- The specific location of each meeting is noted on the notice and agenda for each meeting which is posted at least 72 hours in advance of the meeting. Special meetings may be called as necessary by the Committee Chair and noticed at least 24 hours in advance of the meeting.
- Questions and comments regarding the agenda may be directed to the Public Works Department at (650) 903-6311.
- Interested persons may review the agenda and staff reports at the Public Works Department counter beginning at 4:00 p.m. the Friday evening before each regular meeting. A copy can be mailed to you upon request. Staff reports are also available during each meeting.
- **SPECIAL NOTICE – Reference: Americans with Disabilities Act, 1990**
Anyone who is planning to attend a meeting who is visually or hearing-impaired or has any disability that needs special assistance should call the Public Works Department at (650) 903-6311 48 hours in advance of the meeting to arrange for assistance. Upon request by a person with a disability, agendas and writings distributed during the meeting that are public records will be made available in the appropriate alternative format.
- The Board, Commission, or Committee may take action on any matter noticed herein in any manner deemed appropriate by the Board, Commission, or Committee. Their consideration of the matters noticed herein is not limited by the recommendations indicated herein.
- **SPECIAL NOTICE –** Any writings or documents provided to a majority of the Bicycle/Pedestrian Advisory Committee regarding any item on this agenda will be made available for public inspection in the Public Works Department, located at 500 Castro Street, during normal business hours and at the meeting location noted on the agenda during the meeting.

ADDRESSING THE BOARD, COMMISSION, OR COMMITTEE

- Interested persons are entitled to speak on any item on the agenda and should make their interest known to the Chair.
- Anyone wishing to address the Board, Commission, or Committee on a nonagenda item may do so during the "Oral Communications" part of the agenda. Speakers are allowed to speak one time on any number of topics for up to three minutes.



DRAFT MINUTES

REGULAR MEETING - WEDNESDAY, AUGUST 27, 2014
PLAZA CONFERENCE ROOM AT CITY HALL - 500 CASTRO STREET
6:30 P.M.

1. CALL TO ORDER

Vice Chairperson Purdon called the meeting to order at 6:30 p.m.

2. ROLL CALL

Members Present: Committee members Bruce England, Josette Langevine, Marc Roddin, and Vice Chairperson Simon Purdon.

Member Absent: Chairperson Greg Unangst.

Staff Members Present: Rochelle Kiner, Senior Administrative Analyst; Linda Forsberg, Transportation and Business Manager; and Helen Kim, Transportation Planner.

Public Present: Eight (8) members of the public were present.

3. ORAL COMMUNICATIONS FROM THE PUBLIC

- David Fletcher, resident, suggested bicycle-related improvements at the undercrossing of Old Middlefield Way, a new bike/pedestrian bridge at Colony Street to connect to the trail, and better connectivity to North Bayshore/Palo Alto.
- Aldona Majorek, resident, shared the results of two Cycletopia (protected bikeway) surveys conducted in Mountain View.
- John Carpenter, resident, stated the new sidewalk at Central Expressway, near Moffett Boulevard, was being used by many pedestrians.

4. **MINUTES APPROVAL**

Motion – M/S Roddin/Langevine – Carried 4-0-1; Unangst Absent – Approve the minutes of the May 28, 2014 meeting with the corrections as noted and the June 4, 2014 meeting.

5. **UNFINISHED BUSINESS** – None.

6. **NEW BUSINESS**

6:41 6.1 **BICYCLE TRANSPORTATION PLAN**

The Committee received a presentation and provided input on the City's existing environment for bicycling and opportunities for future improvements. The project consultant (Alta Planning), Transportation and Business Manager and Transportation Planner, responded to Committee questions.

SPEAKING FROM THE FLOOR WITH SUPPORT, CONCERNS, AND/OR RECOMMENDATIONS:

- John Carpenter
- Dave Pilling
- Aldona Majorek
- David Fletcher
- Lada Adamic

7:20 6.2 **UPCOMING AND RECENT EVENTS**

The Committee discussed the members' participation in the Council Neighborhoods Committee meetings and other events.

7:21 6.3 **FISCAL YEAR 2014-15 BICYCLE/PEDESTRIAN ADVISORY COMMITTEE (B/PAC) WORK PLAN UPDATE**

The Committee reviewed its Fiscal Year 2014-15 Work Plan and provided updates on the Committee's recent activities.

7:50 **6.4 VALLEY TRANSPORTATION AUTHORITY (VTA) BICYCLE & PEDESTRIAN ADVISORY COMMITTEE (BPAC) UPDATE**

The Committee received a report from the City's VTA BPAC representative on the VTA BPAC agenda items.

7. COMMITTEE/STAFF COMMENTS, QUESTIONS, AND REPORTS

7:53 **7.1 STAFF COMMENTS**

- Staff distributed to the Committee the Police Department's bike/pedestrian-related accidents data (January to June 2014) and City's suggested routes to school program (grades K-12) data (February to April 2014).
- There were 20 bicycle-/pedestrian-related cases (6 open and 14 resolved) reported in the past three months (June to August 2014) in the City's Customer Relationship Management (CRM) System.
- The Mountain View Community Shuttle Proposed Pilot Program will be presented to the City Council on September 16, 2014.

7:56 **7.2 COMMITTEE COMMENTS**

- A Committee member stated the Los Altos Bicycle Advisory Committee recommended several projects for approval, reviewed several *Ask MV* cases, and announced the City Clerk will distribute information about the North Central County Bus Improvement public meeting to City committees.
- A Committee member shared concerns regarding pedestrian-related car accidents. The Police Department data shows most accidents involve drivers failing to yield to pedestrians and recommended more education for drivers.

In response to a member's inquiry, the Transportation and Business Manager stated staff will check with the Community Development Department regarding the review process for the revised San Antonio Center, Phase II project, including the possibility for additional B/PAC review and comment.

In response to a member's inquiry, the Transportation and Business Manager explained the Bicycle Transportation Plan Update included bicycle connectivity concerns/facilities with neighboring jurisdictions.

8. SET DATE AND TIME FOR NEXT MEETING

Wednesday, September 17, 2014, at 6:30 p.m.

9. CALENDAR

Wednesday, October 29, 2014 – B/PAC Meeting

Wednesday, November 19, 2014 – B/PAC Special Meeting

10. ADJOURNMENT

The meeting was adjourned at 8:02 p.m.

HK/5/PWK
915-08-27-14mn

**MEMORANDUM**

Public Works Department

DATE: September 17, 2014

TO: Bicycle/Pedestrian Advisory Committee

FROM: Lorenzo Lopez, Senior Traffic Engineer
Sayed Fakhry, City Traffic Engineer
Michael A. Fuller, Public Works Director

SUBJECT: Modifications to Castro Street

RECOMMENDATION

Provide input on the proposed modifications to Castro Street as part of Project 14-36, Modifications to Castro Street, between El Camino Real and Miramonte Avenue project.

BACKGROUND AND ANALYSIS

The project area is an approximately 2,000' segment of Castro Street between El Camino Real and Miramonte Avenue, and a portion fronts Graham Middle School. Graham Middle School, located at 1175 Castro Street, is one of two public middle schools serving the residents of Mountain View. Graham Middle School is also the site of the Graham Sports Complex, a youth sports venue jointly operated by the City of Mountain View and the Mountain View Whisman School District.

Castro Street, between El Camino Real and Miramonte Avenue, is a collector street connecting downtown, El Camino Real, and the residential neighborhoods in the southerly half of the City. In addition to being the primary access route to Graham Middle School, Castro Street is also a major access route to Huff Elementary School, approximately one-half mile south of Graham Middle School.

Castro Street is a four-lane collector street with two lanes in each direction separated by a raised median. The posted speed limit is 30 miles per hour. There currently are no bike lanes along this segment of Castro Street. The Average Daily Traffic volume is approximately 6,800 vehicles per day with peak volumes occurring during morning and afternoon commute times that coincide with opening and closing bells at Graham Middle School. By comparison, the Average Daily Traffic volume on Castro Street at Mercy Street is approximately 17,000 vehicles per day.

Staff reviewed Statewide Integrated Traffic Records System (SWITRS) collision data which indicated there were four traffic collisions involving bicyclists (two) and pedestrians (two) between 2006 and 2010 within the project area. In October 2012, there were an additional three accidents involving students biking or walking to Graham Middle School within the project area. These incidents initiated communication between parents, school staff, and the City and, as a result, changes to Castro Street are being proposed.

A survey of Graham Middle School conducted as part of the City of Mountain View's ongoing Vehicle Emissions Reductions Based at Schools (VERBS) noninfrastructure grant education program found that of the approximately 800 students attending Graham Middle School, approximately 200 students walk to school and 30 to 35 ride bikes each day (weather permitting). The survey also revealed that many more students would walk or bike to school if traffic calming improvements and bike/pedestrian-friendly features were added to Castro Street. This survey data is further reinforced by a letter from the Graham Middle School Principal to the City Council supporting a traffic calming project along Castro Street and 49 other letters received from teachers and parents supporting such a project.

The proposed project will improve pedestrian and bicycle safety on Castro Street, between El Camino Real and Miramonte Avenue, and change the character of the street to a Complete Street by adding buffered bike lanes in both directions, reducing vehicular lanes from four lanes to two lanes, constructing bulb-outs and high-visibility crosswalks with in-roadway warning lights (IRWLs), and modifying the intersection geometry and the traffic signal at Castro Street and Miramonte Avenue. The proposed reduction of lanes and bulb-outs will reduce the number of travel lanes for pedestrians to cross and shorten the crossing distance respectively. These improvements will encourage greater levels of walking and biking to Graham Middle School by reducing traffic speeds and providing an enhanced biking and walking environment and experience. A conceptual plan of the proposed changes can be seen in Attachment 1. Funding for construction of the proposed improvements has been received from a VERBS infrastructure grant (\$840,000).

Over the years, studies have been done to determine what Average Daily Traffic (ADT) values would be acceptable when reducing vehicular lanes from four lanes to two lanes with a two-way left turn median which is commonly referred to as a "road diet." There have been cases where the ADT was as high as 20,000 to 25,000 and these roadways were considered candidates for a road diet after they were evaluated for feasibility. Generally, lower ADT values of less than 15,000 have shown to have very good results in the areas of safety, operations, and livability after road diet implementation. The

studies have shown that the ideal ADT for a road diet project is about 12,000 vehicles per day. There are many instances over the last 25 years where the reduction of vehicular lanes from four lanes to two lanes with a two-way left turn median has resulted in the same or even higher ADT after the lane reduction.

The City of Mountain View has implemented road diet projects at three locations in the past which include Cuesta Drive between Miramonte Avenue and Springer Road (ADT = 14,200), Phyllis Avenue between El Camino Real and Grant Road (ADT = 6,400), and Dana Street between Pioneer Way and Calderon Avenue (ADT = 8,000). For comparison, this section of Castro Street currently carries approximately 6,800 vehicles.

As part of the conceptual design of this project, staff visited the intersection of Castro Street and El Camino Real for multiple days to observe the queuing in the northbound direction. During the short 15-minute peak period (7:40 a.m. to 7:55 a.m.) before school started, there was a steady stream of vehicles in the northbound direction and for the majority of the traffic signal cycles every car was able to clear the intersection. The high volume of students crossing El Camino Real on the east side of Castro Street between 7:40 a.m. and 7:55 a.m. and activating the pedestrian signal provided the additional green time needed for these vehicles to clear.

Common traffic engineering practice suggests that a street like Castro Street can typically carry 400 to 600 vehicles per hour per lane. Recent traffic counts showed a peak volume of approximately 400 to 450 vehicles in the northbound direction between 7:00 a.m. and 8:00 a.m. between Miramonte Avenue and El Camino Real. The southbound direction showed a peak volume of approximately 300 to 350 vehicles between 5:00 p.m. and 6:00 p.m. between El Camino Real and Miramonte Avenue. It is also anticipated that if the proposed changes are installed, there may be some northbound Miramonte Avenue traffic that will remain on Miramonte Avenue rather than take Castro Street and could reduce the future volumes.

Because of the history of accidents between vehicles and bicyclists and pedestrians, including the three separate accidents in the project area during October 2012, the Castro Street Modification project has broad support from the Graham Middle School administration and faculty, the school community, and the Mountain View Bicycle/Pedestrian Advisory Committee (B/PAC).

PUBLIC NOTICING

In addition to agenda posting, notices of this meeting were mailed to the residents and property owners along Castro Street and the surrounding side streets that feed into this section of Castro Street as shown on the attached Notification Map (Attachment 2).

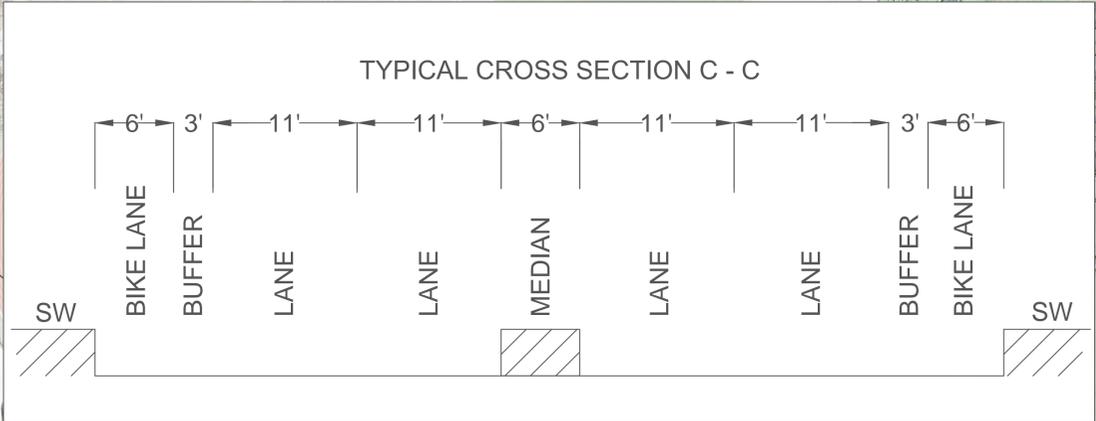
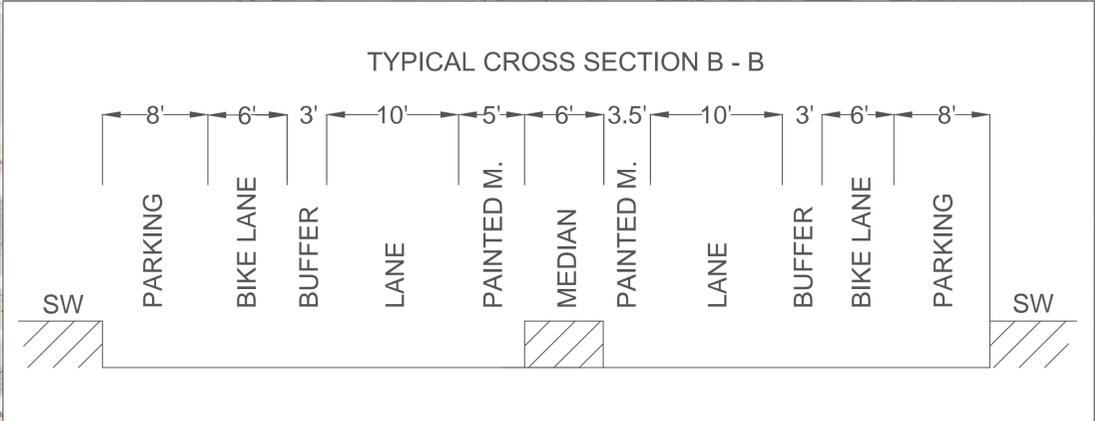
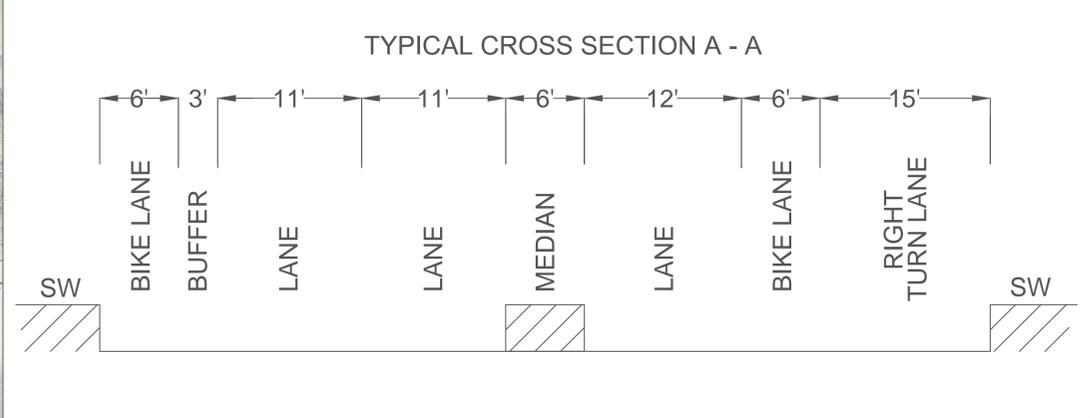
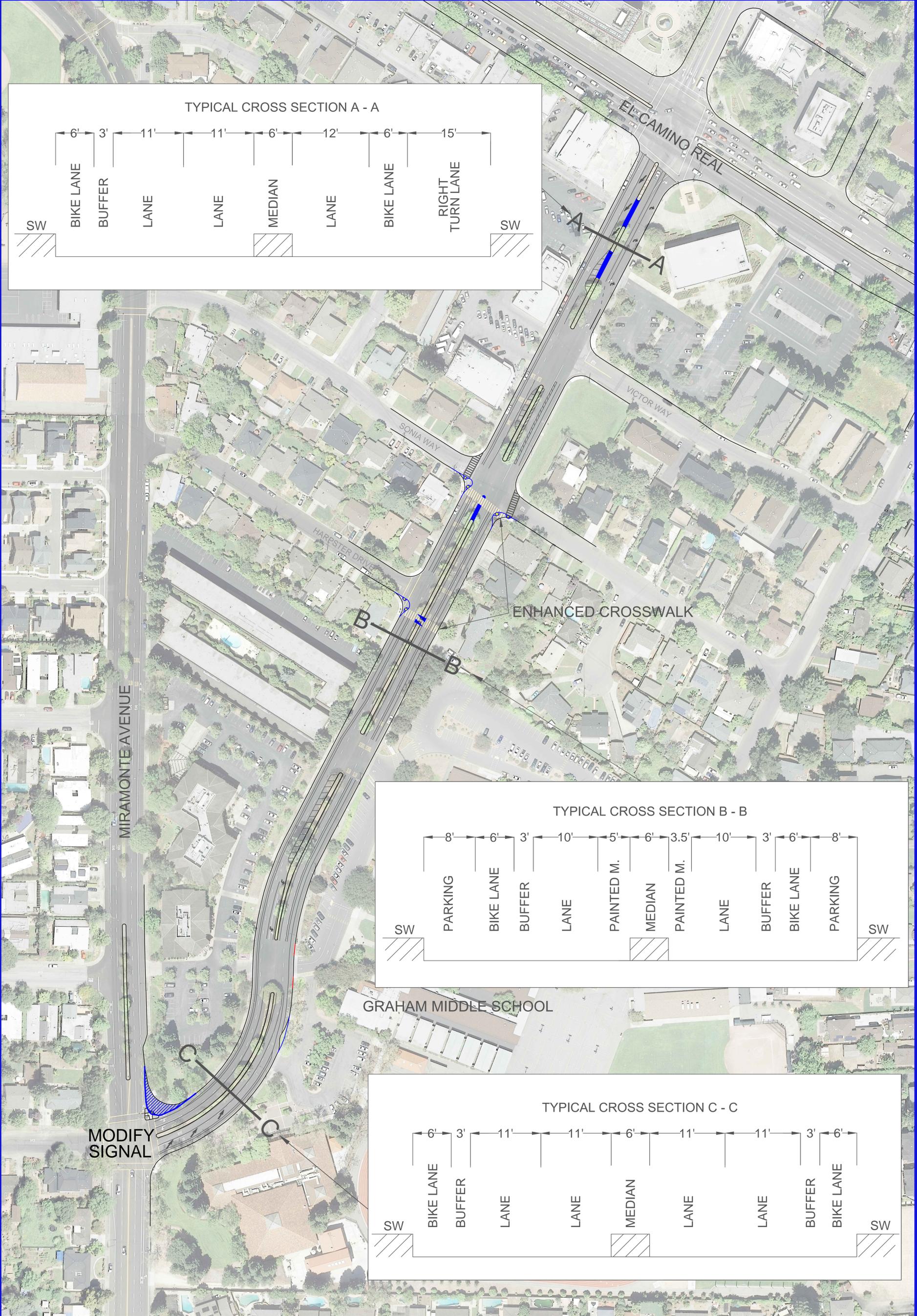
LL-SF-MAF/5/PWK
904-09-17-14M-E

Attachments: 1. Conceptual Plan—Modifications to Castro Street, between El Camino Real and Miramonte Avenue
2. Notification Map

cc: City Council

CM, PWD, APWD—Solomon, TBM, CTE, STE, TP, PC, F/c

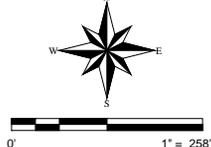
CONCEPTUAL PLAN - MODIFICATIONS TO CASTRO STREET BETWEEN EL CAMINO REAL AND MIRAMONTE AVENUE






Public Works Department
CITY OF MOUNTAIN VIEW
 231 North Whisman Road Mountain View, CA 94043 Santa Clara County
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Attachment 2
 Notification Map
 B/PAC Meeting
 September 2014



0' 1" = 258'

**MEMORANDUM**

Public Works Department

DATE: September 17, 2014

TO: Bicycle/Pedestrian Advisory Committee

FROM: Lisa Au, Principal Civil Engineer
Michael A. Fuller, Public Works Director

SUBJECT: Permanente Creek Trail at Charleston Road – Design Update

PURPOSE

Provide input on the trail design of Permanente Creek Trail at Charleston Road.

BACKGROUND

On May 7, 2013, the City Council considered three alternatives for creating a Permanente Creek Trail crossing at Charleston Road. The alternatives included a bridge, an undercrossing, and an at-grade crossing with a traffic signal. The bridge was significantly more expensive than the other two alternatives, created visual impacts, and required significant tree removal. The undercrossing provided good separation between trail users and vehicles, but was considerably more expensive than the at-grade crossing and required the removal of approximately 20 trees. The at-grade crossing was the least expensive and required the removal of the fewest trees, and was selected by Council primarily for these reasons.

After Council selected the at-grade crossing, Google Inc. (Google) retained a team of landscape architects, civil engineers, and biological consultants to see if an undercrossing alternative could be developed that satisfied Council's concerns about cost and tree removal. Many Google employees use the trail for travelling between company-owned and -leased buildings and for recreational purposes, and Google representatives expressed to staff an interest in making the trail safer and more user-friendly for all trail users by avoiding an at-grade crossing.

On September 20, 2013, the City received a concept from Google for pedestrian/bicycle and landscaping improvements along Permanente Creek from the existing Highway 101 pedestrian overcrossing to the north side of Charleston Road. The concept includes an undercrossing of the Permanente Creek Trail under Charleston Road. The proposal

requires the removal of significantly fewer trees than the alternative considered by Council in May 2013, but eliminated the Santa Clara Valley Water District's (District) maintenance access on the east side of the creek at Charleston Road, which the District previously indicated must be retained. The District agreed to review the proposal.

On May 13, 2014, City Council approved staff's recommendation to delay the City design of the at-grade crossing of Permanente Creek Trail at Charleston Road pending review of Google's undercrossing proposal.

ANALYSIS

Google representatives have met with District and City staff and representatives from the Audubon Society and Citizens Committee to Complete the Refuge to gain input on their proposal, which includes stream channel habitat enhancements and pedestrian/bicycle improvements between Highway 101 and Charleston Road. The District has expressed concern about some aspects of the project, particularly those that increase the District's maintenance effort and cost. The District has agreed to continue to discuss the project with Google and the City in an effort to develop a mutually agreeable project.

In addition to native plantings and other stream channel enhancements, Google's proposal includes a number of pedestrian and bicycle improvements.

Trail Improvements on West Side of Creek and Bridge Improvements

The levee on the west side of the creek is currently surfaced with gravel and provides at-grade access to Charleston Road. Google proposes to surface the trail with an ADA-accessible surface, reconstruct the existing pedestrian bridge that crosses the creek between Amphitheatre Parkway and Charleston Road with a wider bridge, and construct a new pedestrian bridge over the creek between Charleston Road and Highway 101 (see Figure 1).

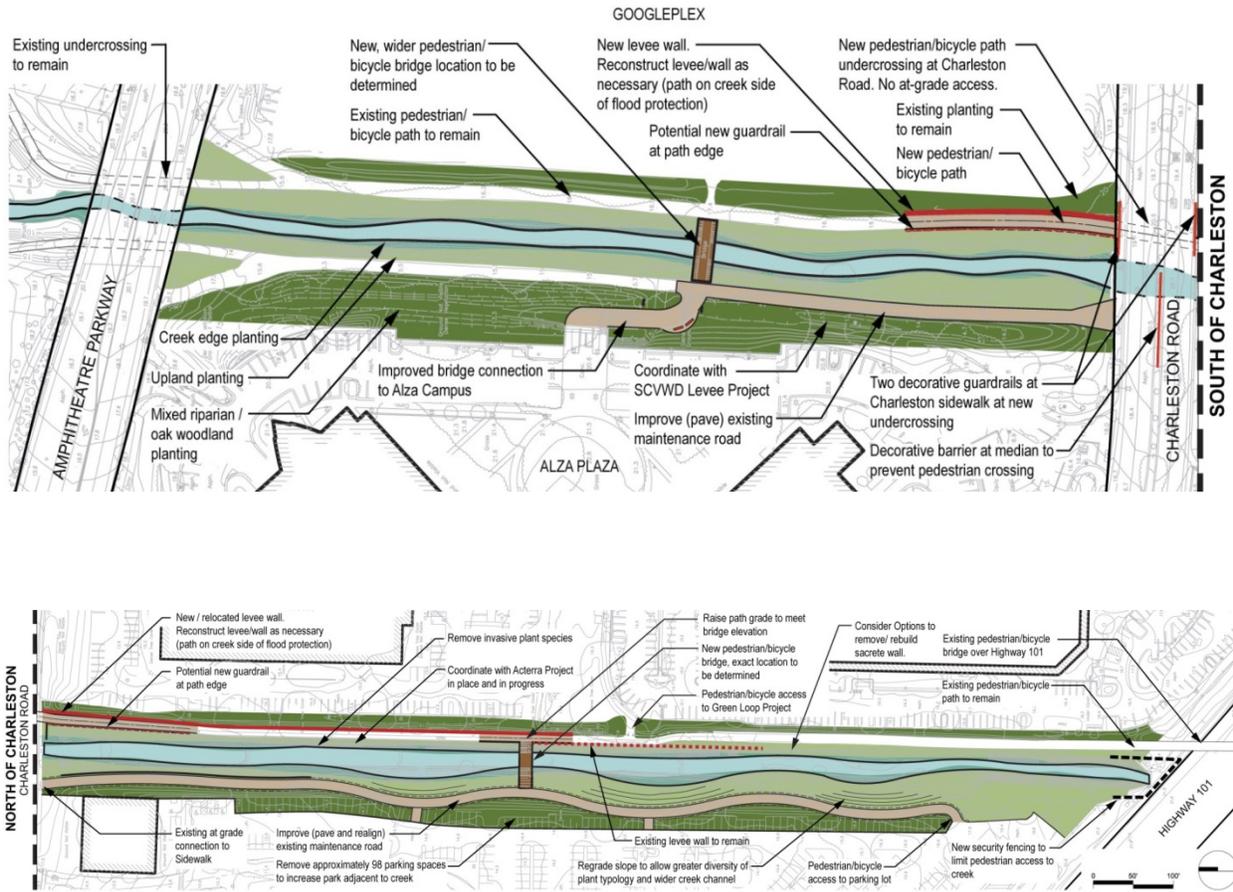


Figure 1 – Proposed Trail Modifications

Google proposes widening the existing bridge because of the high volume of pedestrian and bicycle traffic on the bridge. The new bridge between Charleston Road and Highway 101 would allow trail users to cross the creek to access Charleston Road at-grade (which will no longer be possible with a pedestrian undercrossing on the east side of the creek) and provide pedestrian and bicycle access between the two Google campuses between Charleston Road and Highway 101. The pedestrian bridges would be relatively low-profile, similar to the existing pedestrian bridge over Permanente Creek and to those used along Stevens Creek Trail. A rendering of the new bridge between Charleston Road and Highway 101 is shown on Figure 2.



Figure 2 – Proposed Channel

Charleston Road Pedestrian Undercrossing

Google proposes to construct a pedestrian undercrossing along Permanente Creek Trail at Charleston Road to replace the City’s planned at-grade crossing. To alleviate Council’s concern about tree removals that were required for the undercrossing considered by Council in May 2013, Google proposes to eliminate the District’s maintenance vehicle access at Charleston Road along the east side of the creek. Providing maintenance access would require widening of the excavation for the undercrossing, resulting in additional tree removals. To mitigate the loss of this maintenance access, Google proposes to provide sufficient vertical clearance under the undercrossing for a small maintenance vehicle (up to a pickup truck) to pass under Charleston Road.

Eliminating maintenance vehicle access also eliminates direct access to the trail from Charleston Road. To access the trail from Charleston Road, trail users would use the new trail on the west side of the creek and cross over to the trail via one of the two new pedestrian bridges. Depending on the direction of travel, some backtracking may be required. The route required to access the trail from Charleston Road is shown on Figure 3.

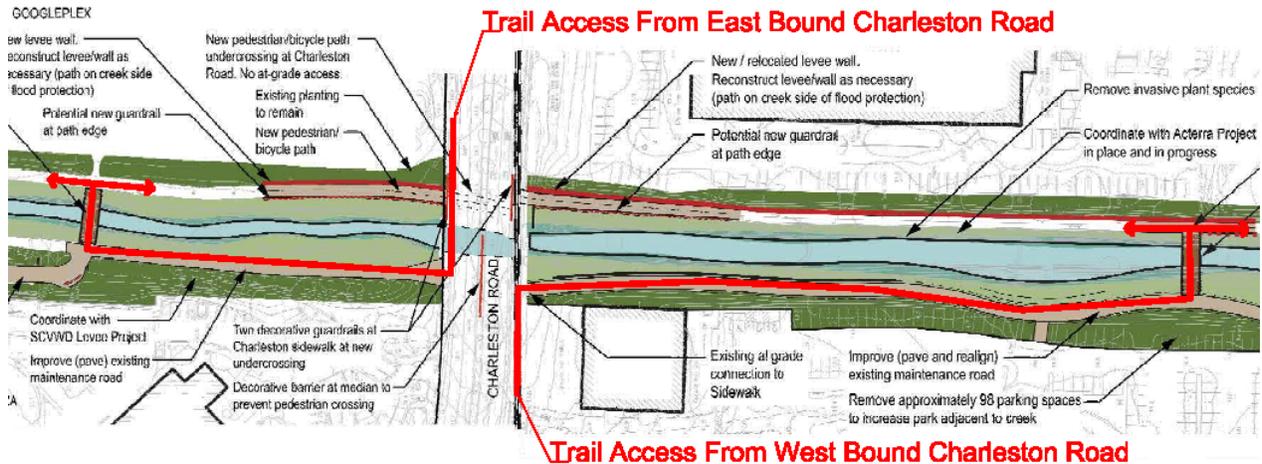


Figure 3 – Access to Trail from Charleston Road

During the May 2014 Council meeting, a number of speakers expressed concern about the loss of direct access to the trail from Charleston Road.

Staff is seeking Bicycle/Pedestrian Advisory Committee (B/PAC) input on the proposed trail concept.

NEXT STEPS

The input received from B/PAC and the community will be forwarded to Google for consideration in finalizing the design and later to City Council with staff's presentation of the final design proposal.

LA-MAF/3/PWK
905-09-17-14M-E

cc: Mr. Tim Smith and Mr. John Igoe, Google Inc.

BICYCLE/PEDESTRIAN ADVISORY COMMITTEE (B/PAC) WORK PLAN

Fiscal Year 2014-15
UPDATED AUGUST 2014

Title and Description	Key Milestones	Date (per milestone)	Current Status Notes
<i>Fiscal Year 2014-15 Work Items</i>			
1. Provide input/direction during Bicycle Transportation Plan update process.	Needs assessment. Draft strategies and recommendations. Draft plan.	September 2014 February 2015 June 2015	B/PAC received project update and provided input on City's existing environment for bicycling on August 27, 2014.
2. Provide input on potential pedestrian improvements along Middlefield Road between the RREEF development at 700 East Middlefield Road and the Middlefield Light Rail Station (carryover from FY 2013-14 Work Plan).	CDD request for B/PAC review/ input.	TBD	

BICYCLE/PEDESTRIAN ADVISORY COMMITTEE (B/PAC) WORK PLAN

Fiscal Year 2014-15

UPDATED AUGUST 2014

Title and Description	Key Milestones	Date (per milestone)	Current Status Notes
3. Promote and participate in events to encourage bicycling and walking.	Farmers' Market. Walk to School Day(s). Multi-Library Bike Tour. Arbor Day. Annual Spring Parade. Bike to School Day(s).	Ongoing, based on B/PAC members' availability. October 8, 2014 October 18, 2014 March 14, 2015 April 2015 May 2015	

BICYCLE/PEDESTRIAN ADVISORY COMMITTEE (B/PAC) WORK PLAN

Fiscal Year 2014-15

UPDATED AUGUST 2014

Title and Description	Key Milestones	Date (per milestone)	Current Status Notes
3. Promote and participate in events to encourage bicycling and walking (continued).	Bike to Work Day. Thursday Night Live. CNC Meetings <ul style="list-style-type: none">• Monta Loma/Farley/Rock Street Neighborhood.• Other neighborhoods.	May 2015 Summer 2015, based on B/PAC members' availability. October 2, 2014 TBD	
4. Annual review of Pedestrian Master Plan (PMP) document.	B/PAC agenda item.	February 2015	

BICYCLE/PEDESTRIAN ADVISORY COMMITTEE (B/PAC) WORK PLAN

Fiscal Year 2014-15

UPDATED AUGUST 2014

Title and Description	Key Milestones	Date (per milestone)	Current Status Notes
5. Monitor PMP performance measures, trends, and targets to assess improvements in the City's pedestrian environment.	Update data on students walking to school.	4 times/ school year	
	Update data on students receiving pedestrian safety education.	4 times/ school year	
	Update data on pedestrian/ vehicle and pedestrian/ bicycle collisions.	Quarterly	
	Update pedestrian safety- related vehicle enforcement measure data.	Quarterly	
6. Receive report and provide input regarding the Police Department's procedure for collecting and reporting data on bicycle and pedestrian-related collisions.	TBD	TBD	To be determined by Police Department staff.
7. Receive report and provide input regarding the availability of bicycle facilities at City-sponsored events.	TBD	TBD	To be determined by CSD staff.
<i>Ongoing Work Items</i>			
A. Review, prioritize, and recommend bicycle and pedestrian projects for annual TDA Article 3 funding application cycle.	VTA call for projects announcement.	February 2015	

BICYCLE/PEDESTRIAN ADVISORY COMMITTEE (B/PAC) WORK PLAN

Fiscal Year 2014-15
UPDATED AUGUST 2014

Title and Description	Key Milestones	Date (per milestone)	Current Status Notes
B. Provide input into the development and review of comprehensive bicycle/pedestrian facility plans and regulations (e.g., General Plan Circulation Chapter, Precise Plans, Zoning Ordinance bicycle parking requirements).	As required/requested.		
C. Review City roadway system and bikeway/pedestrian facilities for bicycle and pedestrian suitability and make recommendations on improvements.	Ongoing.	Ongoing.	<i>Ask MV submitted for bike lane restriping at South Whisman Road/Dana Street.</i>
D. Make recommendations on capital improvements to bicycle/pedestrian facilities.	Annual Capital Improvement Program (CIP) development and approval process.	Spring 2015	
E. Review private development project applications requiring General Plan, Precise Plan, and/or zoning amendments (Gatekeeper projects).	As directed by City Council.		
F. Review public projects to ensure adequate consideration of the needs of bicyclists, pedestrians, and the disabled.	As identified by staff.		
G. Promote bicycle and pedestrian safety via the City website and programs (e.g., VERBS).	Ongoing.	Ongoing.	

BICYCLE/PEDESTRIAN ADVISORY COMMITTEE (B/PAC) WORK PLAN

Fiscal Year 2014-15

UPDATED AUGUST 2014

Title and Description	Key Milestones	Date (per milestone)	Current Status Notes
H. Coordinate with City departments and advisory bodies, other Santa Clara County jurisdictions, and transportation-related agencies (e.g., VTA, Caltrans) on pedestrian and bicycling matters.	City representative attend monthly VTA BPAC meeting. Pursue as opportunities are identified.	Ongoing. Ongoing.	