Sylvan Dale

AN AREA PLAN OF THE CITY OF MOUNTAIN VIEW

SYLVAN-DALE AREA PLAN

One of a series of Derivative Plans
which are part of the City of Mountain
View's General Planning Process

Prepared by staff of the Mountain View Planning Department

Adopted: May 20, 1974 – Resolution No. 10081

Amendments:

1. Resolution 10288
   November 12, 1974

2. Resolution 10365
   January 28, 1975

3. Resolution 11297
   December 27, 1976

4. Resolution 12918
   January 14, 1980

5. Resolution 12982
   March 3, 1980

6. Resolution 13240
   October 13, 1980

7. Resolution 13258
   November 11, 1980

8. Resolution 13627
   June 8, 1982

9. Resolution 13651
   July 27, 1982

10. Ordinance No. 04-05
    March 16, 2005

NOTE: This copy has been updated to include all of the policy amendments through March 16, 2005. However, unless noted, all of the tables, charts and maps are original and may not contain the most recent data.

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LIST OF TABLES AND FIGURES

SYNOPSIS OF THE PLAN

The Sylvan-Dale Plan is a detailed, comprehensive document providing guidance to the policies and programs which will affect the future of the Sylvan-Dale planning area. The plan is "midrange," covering approximately a ten-year period. It is based upon adopted City-wide goals as well as goals developed within the planning area itself.

The principal thrust of the plan is to ensure that the area develops into a stable, comfortable residential neighborhood, offering diverse housing types, quality design, a pleasant, desirable living environment, and opportunities for a range of local urban activities.

Land Use

The land use section of the plan places emphasis on creating opportunities for low- and medium-density residential uses where a quality living environment can be achieved. Densities ranging from approximately 5 to 12 dwelling units per acre are emphasized. Options for both planned unit development (PUD) and conventional development subdivision are available. Because of the desire to enhance ownership potentials and family accommodations, the planned approach at each of these densities is recommended. The housing mix which will result will range in type from very high-density, high-rise apartments to large-lot single-family homes. The plan stresses the need for mixture, recognizing that failure to provide for the diversity of opportunities will weaken the ability to achieve the goals for ownership and family accommodations, and reduce both the number and quality of residential opportunities. The land use section seeks a balance of public facilities, open space and commercial activities to serve the population.

Circulation and Transportation

The circulation section is designed to enhance the basic residential character. Where possible, the heaviest traffic movements are oriented to the periphery of the residential areas where roads can better handle the flow of traffic and adjacent, heavier uses can better tolerate the noise, pollution and hazards of traffic. Buffers, pedestrian crossings and orientation of adjacent uses all seek to enchance the relationship of the automobile facilities to adjacent land uses.

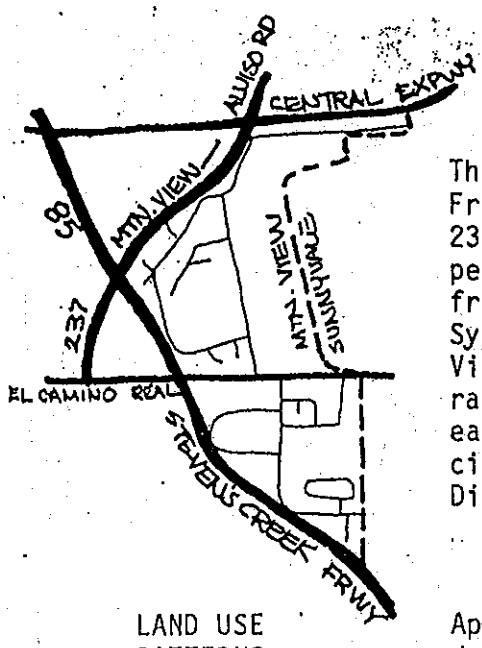
In addition, the design of the circulation system places equal emphasis on meeting pedestrian and bicycle needs as well as traffic requirements. The plan envisions an increasing demand for alternatives to the automobile within the forthcoming decade.

Urban Design

The final major section, urban design, seeks primarily to coordinate uses and activities in order to give a sense of place, identity and an enjoyable environment. Guidelines are provided for both public and private development which will physically blend separate components in the area and avoid harsh conflicts. Emphasis is also placed on means of overcoming isolation of this section of the community from the remainder of the City.

SECTION 1: BACKGROUND

The following section presents the results of the inventory of data available for the Sylvan-Dale area as it presently exists (in 1973 when the plan was originally prepared).



LAND USE PATTERNS

DESCRIPTION OF THE SYLVAN-DALE AREA - 1973

The Sylvan-Dale Area is bounded on the west by Stevens Creek Freeway (Highway 85), Mountain View-Alviso Freeway (Highway 237), to the north by Evelyn Avenue and along the eastern perimeter by the Mountain View-Sunnyvale boundary. The freeways form distinct physical barriers separating the Sylvan-Dale Area from the remainder of the City of Mountain View. The City limits, while not providing the physical separation afforded by the freeways, provides the boundary on the eastern side of the area. This easterly boundary is also coincidental with the limits of the Mountain View Elementary School District which serves this portion of the community.

LAND USE

Approximately 41.8 percent of the land area is devoted to residential uses, 18.4 percent to commercial and industrial uses, and 39.8 percent to other uses, including nonurban, agricultural, vacant, and street areas. Table I details the results of a 1973 land use survey and is accompanied by Figure A, a land use map on the following page.

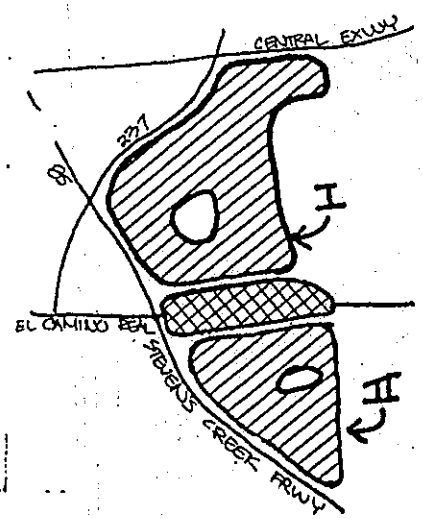
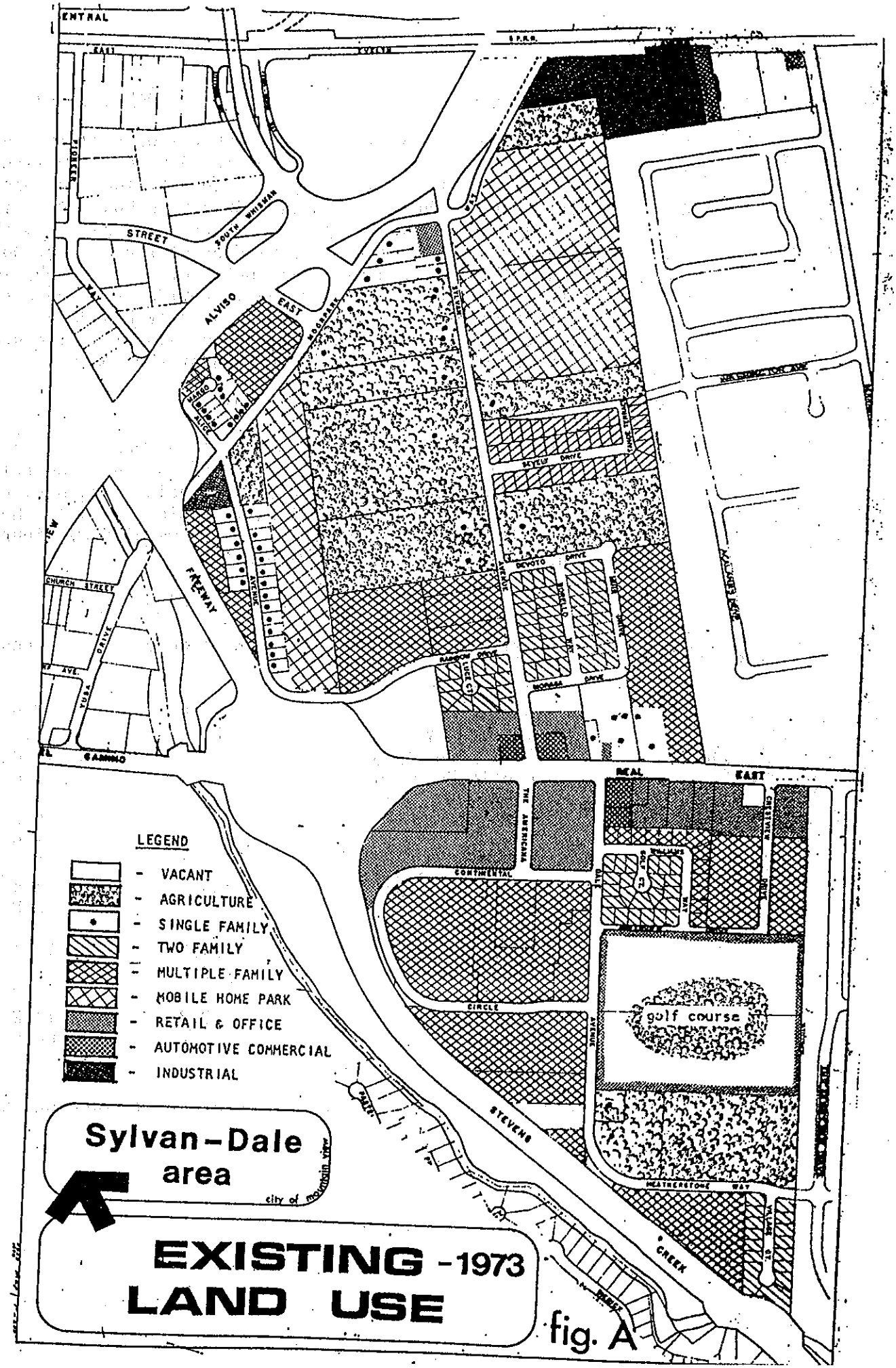


TABLE I - EXISTING LAND USE

Existing Land Use	Total Acres	Percent
Residential		
Single-family	9.0	2.6
Two-family	25.7	7.6
Multiple-family	67.2	19.8
Mobile Home Parks	39.4	11.8
Commercial & Industrial		
Retail/Office	18.7	5.5
General	5.2	1.5
Automotive	4.1	1.2
Commercial Recreation	25.0	7.4
Industrial	9.6	2.8
Other		
Agriculture	54.8	16.1
Greenhouses	7.8	2.3
Vacant	24.5	7.2
Street	48.2	14.2
	339.2	100.0

Based on March 1973 survey, Mountain View Planning Department.


LEGEND
- VACANT
- AGRICULTURE
- SINGLE FAMILY
- TWO FAMILY
- MULTIPLE FAMILY
- MOBILE HOME PARK
- RETAIL & OFFICE
- AUTOMOTIVE COMMERCIAL
- INDUSTRIAL
Sylvan-Dale area
city of mountain view
EXISTING -1973
LAND USE
fig. A
golf course
ALVISO STREET
EAST
SOUTH WHISMAN
FREEWAY
CHURCH STREET
EL CAMINO
REAL
EAST
STEVENS
CREEK
CIRCLE
CONTINENTAL
THE AMERICANA
HEATHERSTONE WAY
DEVOTO DRIVE
SKYLINE DRIVE
MORGAN DRIVE
RANDOM DRIVE
TULSA DRIVE

The existing uses provide the basic form of the neighborhood. El Camino serves as a central commercial core providing both neighborhood and regionally oriented shopping facilities. On either side of El Camino lie the two residential areas, each of which is to have a central public or quasi-public open use.

RESIDENTIAL

MULTIPLE-FAMILY

The existing residential development is predominantly multiple-family in nature. As depicted in Table II, 81 percent of the existing dwelling units are multiple-family and house an estimated 80 percent of the population. The majority of this multiple-family development is in new, large-scale developments. As an example, 7 apartment complexes have over 100 units each and account for 1,325 units, approximately 55 percent of the total number of housing units in the area.

TABLE II - RESIDENTIAL DEVELOPMENT

	Number of Units	Percent
Single-family	51	2
Multiple-family		
2-4	313	13
5+	1,639	68
Mobile Homes	422	17
TOTAL	2,425	100

MOBILE HOMES

The second largest group of residential dwelling types is mobile homes. Three mobile home parks are located in the area north of El Camino Real. The parks each have approximately 140 units and an average density of 10.8 units per net acre. These developments, like the larger apartments, are isolated units or subneighborhoods of the area. None of the mobile home parks permit families with children.

"DUPLEXES"

The third major division of dwelling types may be characterized as duplex subdivisions. The nature of their placement in the area also divides these developments into subunits distributed throughout the area. The "duplexes" have an average density of approximately 10.9 units per acre.

SPECIAL QUARTERS

Only two accommodations for group living are provided within the area. Both of these are residential care facilities for the elderly and are located within duplexes. There are no convalescent facilities, care facilities for the physically and mentally handicapped, or other special dwelling types.

OWNERSHIP

Approximately 30 percent of the 145 R2 lots are held by absentee (investment) owners. The mobile home owners all lease the land on which their homes are situated.

Both mobile homes and duplexes provide a sense of home ownership within the area.

Only 51 dwelling units within the area are single-family, and of this number 30 may be considered permanent with respect to zoning policy. The others occupy land zoned for commercial or higher-density residential uses.

The remainder of the housing, an estimated 95 percent of the 2,425 units, is in rental occupancy. None of the apartments have converted to condominium ownership.

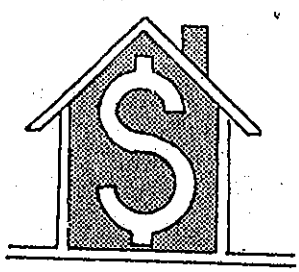
DETERIORATION

Very little evidence of blight or deterioration is evident in the housing within the area. A minor "decline" may be observed in several small single-family homes and an apartment complex located on Moorpark Way. Some signs of neglect are evident. The single-family structures involved are small, older structures, but are apparently sound, based on a recent housing survey. The apartment, which has poor access and is located adjacent to the freeway, shows signs of heavy use. It is one of the few apartments in the area which offers accommodations for children. The newness of the overall area makes housing deterioration a minor concern at present.

SENSE OF PARTICIPATION

It is observed that when assembled together, these residential types provide little sense of neighborhood or community. Each development is almost an independent residential unit. While there was a diverse input at the community meetings preceding development of this Area Plan, it was quite evident that the number of persons active from the duplex residential areas far exceeded the proportionate number living in the area. Neither tenants of the apartments or duplexes nor residents of the mobile home parks showed appreciable interest in the meetings. While this may partially be accounted for by notification procedures, that alone is inadequate to describe the differences in attendance.

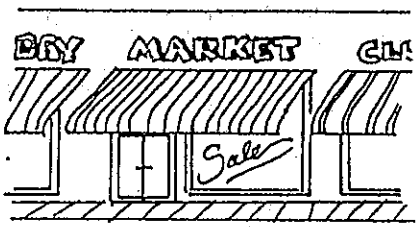
COST OF HOUSING



The cost of housing appears, from available data, to be generally higher than the City as a whole. According to data from the 1970 census, the median cost for owner-occupied single-family units only for the Sylvan-Dale Area was $28,300, compared to a City-wide median of $28,000. However, with respect to rentals, the 1970 census indicated the median rental within the Sylvan-Dale Area of $182, compared to a City-wide median of $169. The 1970 census data is prior to the construction and rental of most of the newer apartments within the area. A 1973 survey conducted by the Planning staff indicated an overall average rental price of $212 for multiple-family units within the area. This ranges from a low of $135 for a studio apartment to a high of $360 for a three-bedroom apartment. It is estimated that the average duplex rental unit cost exceeds the average apartment. These housing cost figures do not include the 422 units of mobile homes which have a special combination of rental and ownership arrangements.

There are few permanent housing opportunities for low- and moderate-income families in the Sylvan-Dale Area. No federally subsidized developments are located within this part of the City of Mountain View.

COMMERCIAL



Commercial use within the Sylvan-Dale Area is concentrated along El Camino Real. Unlike the typical strip commercial uses found along El Camino, many of those within the Sylvan-Dale Area provide neighborhood level services. The newly constructed market and attendant shops at El Camino and The Americana, as well as the small center to the north of El Camino, adjacent to Sylvan, provide neighborhood service facilities. Other neighborhood shopping complexes are located to the east in Sunnyvale and to the east adjacent to Grant Road in Mountain View. The Emporium is located within easy walking distance of most of the residents within the Sylvan-Dale Area.

In addition to the commercial uses along El Camino, a 7-Eleven store is located at the northerly end of Sylvan Avenue. This facility provides limited convenience shopping to the area. Additional commercial facilities are under construction at the corner of Bernardo and Evelyn but will do little to serve residents of the Sylvan-Dale Area.

EMPLOYMENT

In addition to the commercial facilities, a strip of industrial uses located adjacent to Evelyn Avenue provides employment opportunities. Just outside the Sylvan-Dale Area, along the extension of Dana Street, are numerous industrial uses. Because of the relationship to the freeways, access to major industrial areas at Lockheed, Ellis-Middlefield Industrial Park, and industrial locations in Sunnyvale is immediately available to residents of this area.

GOLF COURSE

A major community-wide commercial recreation facility, the Cherry Chase Golf Course, is located on 25 acres of land south of El Camino Real. The golf course is the only public or quasi-public open space presently existing in the Sylvan-Dale Area. The use provides a balance of recreation opportunities not only within the Sylvan-Dale Area but in the community as a whole.

PUBLIC LAND

Approximately 47 acres of land within the Sylvan-Dale Area are publicly owned. Of that, 4 acres at the corner of Dale and Heatherstone are owned by the City of Mountain View for intended park uses and the remainder, owned by the Mountain View Elementary School District, is land bought several years ago for school construction. All of the public land is presently vacant or in agricultural uses. There is considerable uncertainty as to the future need for the District-owned land for school purposes.

PRIVATE VACANT LAND

Approximately 26 acres of private land is vacant or in transitional agricultural uses. Virtually all of this land lies north of El Camino in the Sylvan area.

"MISSING USES"

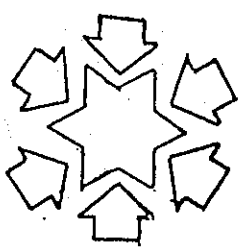
There are many uses "missing" from the Sylvan-Dale area. As was mentioned earlier, there are few facilities for group living. There are presently no public facilities such as parks, schools, libraries, fire stations, etc. There are no quasi-public service structures such as churches or lodge halls. Nor are there any medical facilities in close proximity. A single preschool is located on Moorpark Way but is oriented more towards residents from outside the area than to immediate neighborhoods.

TRANSPORTATION AND CIRCULATION

The circulation facilities define the shape and function of the land uses within the urban area. In the Sylvan-Dale Area, the freeways have already provided the basic limits and form to the neighborhood and established a visible and physical barrier to the rest of the community. These freeways also serve as important links to employment and recreational activities.

Within the Sylvan-Dale Area, 14.2 percent of the land or 48.2 acres are developed with local streets (does not include the surrounding freeways).

ACCESS

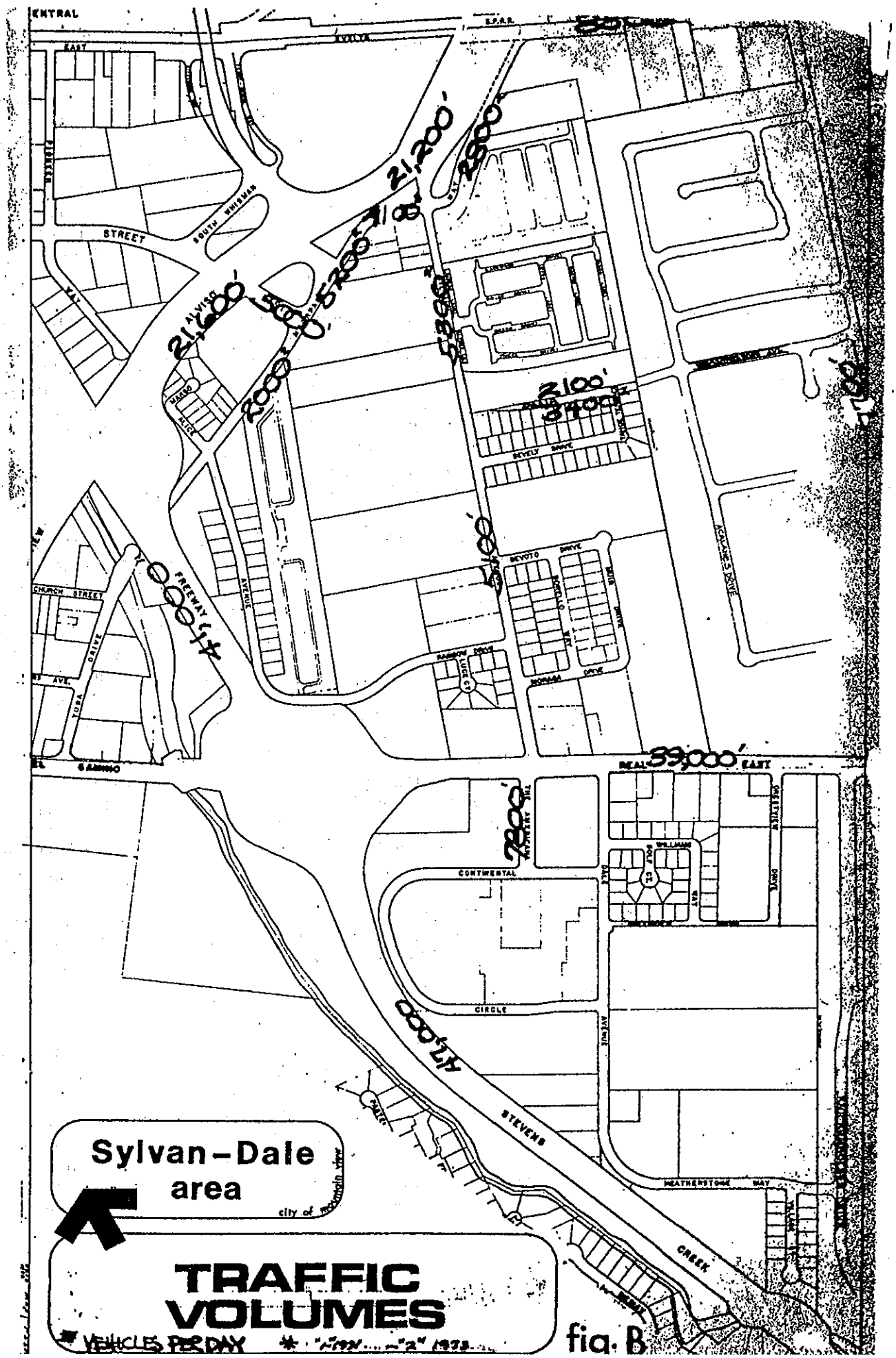


Access to the Sylvan-Dale Area from the surrounding commercial and employment centers as well as other residential areas of the City is excellent. Likewise, residents of this area have immediate access to a major freeway system as well as Evelyn Avenue, Dana Street, and El Camino Real for access to local activity areas.

While automobile access to and from the area is excellent, access for pedestrians and bicyclists is particularly bad. The freeways and heavily traveled El Camino serve as barriers to local pedestrian and bicycle movements. Those pedestrian facilities which are provided are presently disjointed and designed as secondary elements to the streets.

TRANSIT

A limited amount of transit serves the Sylvan Area with Greyhound lines running along Evelyn Avenue and El Camino on an alternating schedule and the Santa Clara County interim system traversing Dana Street, Sevely, Sylvan, and Moorpark. Present transit plans call for implementation of demand-responsive service (dial-a-bus) and a County-wide fixed route arterial system which would serve the area along El Camino.


Sylvan-Dale area
TRAFFIC VOLUMES
VEHICLES PER DAY
fig. B
STREET
SOUTH WHISMAN
ALVISO
HARDO
BEVERLY DRIVE
BEVOTO DRIVE
CHURCH STREET
FREEWAY
CONTINENTAL
CIRCLE
STEVENS
CREEK
HEATHERSTONE WAY
MEAL 39,000' EAST
21,200'
21,600'
5200'
2000'
5300'
2100'
3400'
7200'
41,000'

DESCRIPTION OF EXISTING STREETS - 1973

El Camino Real

El Camino Real is a 120' wide, six-lane arterial under State of California jurisdiction. Improvements for the completion of the street to its full six-lane capacity with divider islands will be completed by 1974 from the freeway interchange easterly through Sunnyvale. El Camino is the major east-west connector in the Sylvan-Dale Area. Signalized intersections are located at the Americana-Sylvan intersection and Bernardo Avenue. This street, when completed, will be fully landscaped and will have 8' wide sidewalks.

Evelyn Avenue

Evelyn Avenue is planned to be an 86' wide, four-lane arterial through this section of the community. It provides a flow of traffic from Bernardo to Moorpark to tie into the freeway and may also provide access to the central part of the City. Improvements to Evelyn are tentatively scheduled for 1978-83 at a cost of approximately $781,000. The street is now operating as a two-lane arterial serving the commercial and industrial uses along Evelyn Avenue. A sidewalk will be provided along the southerly side of Evelyn at the time of completion.

Sylvan Avenue

Sylvan is proposed as a 70' wide minor arterial extending from El Camino to Moorpark Way and the entrance to Mountain View-Alviso Freeway. The street is presently unimproved along much of its length. The standard 70' width would allow for two lanes of trafic. No special design attention has been given to pedestrian ways or bike lanes at the present time. Normal monolithic sidewalks would be included in the standard street design. The street has been designated within the General Plan for a bike path.

The Americana

The Americana is a 90' wide modified commercial street with four lanes divided by a median and a special right-turn lane for the Emporium complex. The Americana, along with a portion of Continental Circle, provide direct access from El Camino to Dale Avenue and Heatherstone Way via the signalized intersection. A bike path is planned for this section of roadway.

Dale-Heatherstone

Dale-Heatherstone is a 60' wide residential street with completed roadway throughout its length. The street serves as access from El Camino to Bernardo. The sidewalk sections are complete, with the exception of the golf course frontage. A bike path is designated for this street.

Dana Street

Dana Street, east of Sylvan, has a recently adopted Precise Plan for a basic 60' residential street that widens to 90' as it nears Sylvan, replacing the former 90' row throughout. The southerly half of the street was improved as part of the subdivision. The street carries a relatively high volume of traffic from lands to the east, along Washington Avenue in Sunnyvale. A Precise Plan has been adopted for a four-lane, 90' wide street traversing from Moorpark to Sylvan.

Moorpark Way	The section of Moorpark, north of Dana Street, is a frontage road extending to Evelyn Avenue. Sidewalks are incomplete and need to be established along the length of the southerly side of the street.
Alice Avenue	Much of the land along Moorpark and Alice Avenue, south of Dana, is unincorporated. The roadway is generally good, although not developed to City standards. Curbs, gutters, and sidewalks are not provided in most of the street. The street joins Rainbow Drive, a fully improved 60' street.
Greenview Drive	Greenview is a partially-improved residential street which abuts the golf course. The golf course frontage is unimproved.
Other Streets	The remainder of the public streets are standard 60' residential streets providing access to adjacent uses. Those streets serving the mobile home parks internally are private roads.
LEVEL OF SERVICE	Traffic engineers rate the traffic carrying capacity of arterial streets and highways by a level of service evaluation which includes factors of speed and the ratio of demand to capacity. There are six levels of service ranging from A to F. The first four levels, A through D, are conditions where demands are acceptably met. A is a completely free-flowing movement, while D provides marginal driving freedom and comfort. All of the arterial streets in the Sylvan-Dale Area operate at Level C or above, even at peak times. The level of service is effective, even though some of the streets such as Sylvan are not fully improved.
PROBLEMS	Much of the traffic in the area is "through" in nature, neither beginning nor terminating within the Sylvan-Dale Area. One area of particular concern to the residents is the traffic generated along Heatherstone, Sylvan, and The Americana, particularly during commute hours. This series of streets apparently serves as a convenient short-cut to avoid the signalization along El Camino. The nature of the streets is such that traffic appears to travel at a relatively high rate of speed and the street and its attendant traffic tend to divide the neighborhood. A second concern is Sylvan Avenue, from El Camino to the freeway: although unimproved through much of its length, it serves as a heavy carrier of traffic from El Camino to the freeway system. It, too, serves to divide the two portions of the Sylvan-Dale Area. A substantial amount of traffic originates from the Bernardo Avenue Area in Sunnyvale, where Bernardo and Washington are signalized, traverses Washington and Dana and intersects at Sylvan. This provides access to the freeway and also access to industrial areas on the opposite

side of Route 237. Both El Camino and Evelyn can provide the same access.

Improvement of streets and addition of streets which would increase this through traffic problem will jeopardize the residential quality of the area. Future improvements to Sylvan and Dana Street will have this effect.

PEOPLE - 1973

An estimated 4,440 persons or 7 percent of the total Mountain View planning area population live within the Sylvan-Dale Area. As evidenced in Table III, 62 percent live in apartments, 18 percent in mobile homes, and another 18 percent in duplex structures. Less than 2 percent live in single-family structures.

TABLE III - ESTIMATED POPULATION

	No. of Units	1968 Family Size Sylvan-Dale Area	Est. Pop.	% Total Pop.
Single-family	51	2.00	102	2%
Multiple-family				
2-4 family	313	2.07	793	18%
5+ family	1,639	1.76	2,743	62%
Mobile Homes	422	1.90	802	18%
TOTAL	2,425	overall 1.90	4,440	100%

*Source: 1968 Insight.

AGE

As may be expected from this housing situation, the existing population is distinctly different from the City average. The following tables compare age distribution characteristics of the Sylvan-Dale Area to the City as a whole.

INCOME

Income data for the area is scarce, both from local data and the census. Census data covers both the Sylvan-Dale and Whisman Areas within the same census tract. This census tract as a whole had a median income of $11,042 per family and $7,354 for unrelated individuals; the median income of all households being $8,942, compared to City averages of $11,830, $6,709 and $9,364 for the same groups City-wide. Thus, for the entire census tract there is a characteristically lower income. However, it is the assumption based on other indications in the census that the Whisman Area assumes an unequal proportion of the lower incomes and that the Sylvan-Dale Area enjoys approximately the same median income as the City as a whole, even with the substantial number of older individuals found there.

MINORITIES

TABLE VII - MINORITY DISTRIBUTION (1970 CENSUS)

	City Planning Area	Sylvan-Dale	Whisman	Sylvan-Dale Plus Whisman
Black	1%	0.4%	2.5%	1.6%
Other*	5%	2.6%	10.7%	7.1%
Span.-Amer.	13.1%	Not Known	Not Known	17.5%

*Includes principally Chinese, Japanese and other Orientals.

It is evident from the above chart that the Sylvan-Dale Area has a lower-than-average minority population for black and other minorities. The same low representation is apparently true for Spanish-Americans.

URBAN ACTIVITY

The Sylvan-Dale Area provides only limited resources for daily activities. Employment, recreation, entertainment and public facilities must, for the most part, be found outside the neighborhood area. Each of these demands in turn generate additional automobile traffic--and little else.

The nature of existing residential development tends to isolate activity within each development unit. Each apartment, each mobile home park, and each R2 area is a separate entity with little relationship to the neighborhood as a whole. Many of the subunits provide opportunities for recreation and entertainment, much of it consisting of structured programs for the tenants. There is little interaction, however, outside of the area, partly because of a lack of physical facilities or opportunity in the neighborhood: no parks, no meeting areas, few facilities. As the Sylvan-Dale Area is isolated from the remainder of the City, so each of the major residential components of the area is isolated from each other.

The population has a limited number of children and middle-range family ages and a high number of young adults and older citizens. This may be accounted for by the nature of the living accommodations, rental policy, housing cost, and adequacy fo public facilities, i.e., the lack of schools or parks.

If the 1968 averages are applied, 220 kindergarten through sixth-grade children could be expected to be found in the existing housing. During 1973, only 97 children in the area attended public elementary schools, all bussed to either Landels, Huff or Cooper. The lack of public facilities, particularly schools and parks, may account for the low number of school-age children. Secondly, the rental policy of the apartments and mobile homes is such to restrict families with children. A minor change in the management policy of apartments could have a dramatic effect on the school-age population. Available evidence shows that two-bedroom apartments for families support a kindergarten through sixth grade age group population equal to those of the single-family average for the City. These considerations have an important impact on policy for public facilities, particularly schools.

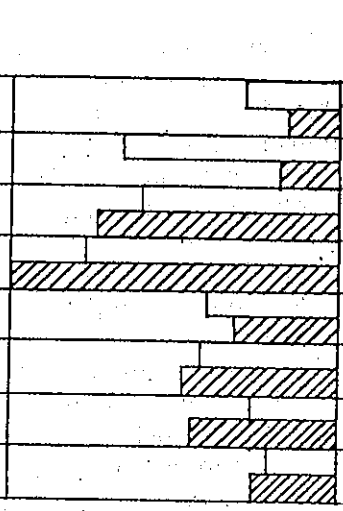
FAMILY SIZES

Family sizes are significantly smaller than the remainder of the planning area. This is accounted for partially by the mobile home parks, partially by the lack of public facilities, and by the rental policies. Based on 1968 family size data, if the area approached the City-wide family size average, the population would rise 25 percent to 5,250 persons in existing housing.

TABLE VI	FAMILY SIZE		NUMBER IN LABOR FORCE	
	Sylvan/Dale	Planning Area		City
1 Family	2.00	3.55	2.00	1.23
2 Family	2.67	3.07	1.33	1.31
3-4 Family	1.75	2.39	1.25	1.28
5+ Family	1.76	2.10	-	-
Average	1.90	2.76	1.48	1.28

Source: 1968 Insight

TABLE IV - AGE DISTRIBUTION



AGE RANGE	SYLVAN-DALE AREA	CITY WIDE
0-4	4.6%	8.0%
5-17	5.0%	18.2%
18-24	20.4%	16.9%
25-34	28.0%	21.5%
35-44	8.5%	11.0%
45-54	13.2%	11.3%
55-64	12.9%	7.1%
65+	7.4%	6.0%
	100.0%	100.0%

TABLE V - AVERAGE NUMBER OF CHILDREN PER DWELLING UNIT - (1968 Insight)

Dwelling Type	SYLVAN-DALE					PLANNING AREA				
	Preschool	K-6	Jr. High	High	Total	Preschool	K-6	Jr. High	High	Total
1 Family	-	-	-	-	-	0.40	0.56	0.17	0.32	1.45
2 Family	0.50	-	0.17	-	0.67	0.69	0.24	-	0.05	0.98
3-4 Family	-	-	-	-	-	0.36	0.16	-	0.09	0.61
5+*	0.10	0.07	0.10	-	0.27	0.19	0.06	0.03	0.04	0.32
All	0.08	0.08	0.08	-	0.32	0.31	0.29	0.08	0.16	0.84

*Averaged for all 5+ unit types

SECTION II: GOALS

Development of an area plan must be based on both community-wide and local goals. Community-wide goals have been developed during the Search Program and adopted by the City. Local goals were pursued through a series of neighborhood committee meetings with Sylvan-Dale Area residents. For the most part, these two levels of goals are consistent with each other.

The majority of the goals center around the basic concept of assuring the creation of a residential neighborhood of sound design and character.

GOALS

The following goals are the basis for the plan which follows:

1. Community Balance: Develop a neighborhood which incorporates a variety of residential uses, recreation opportunities, public facilities, and commercial services to serve its population.

2. Residential Balance: The Sylvan-Dale Area shall provide residential accommodations for a diverse population in terms of age, income, ethnic background and family composition.

3. City Links: Ensure that the Sylvan-Dale Area is physically, visually and socially tied to the remainder of the City.

4. Unification: Assure that remaining public and private development serves to tie the Sylvan-Dale Area visually, physically and socially together.

5. Visual Design: Create a basically residential area of diverse but coordinated design character.

6. Ownership: Provide increased opportunities for home ownership within the Sylvan-Dale Area.

7. Neighborhood Facilities: Assure that adequate public open space, recreation and service facilities are available to the residents of the area.

8. Civic Responsibility: Increase the opportunity for involvement of citizens of the area in the governmental decisions affecting the entire City.

OBJECTIVES

From the basic goals stem the following specific objectives:

a. Maximize accommodations for families with children.

b. Optimize ownership potential within all densities.

c. Introduce lower densities within the Sylvan-Dale Area.

d. Encourage utilization of innovative, residential planning design which provides accommodations for families and persons of all incomes.

e. Provide opportunities for all incomes within the Sylvan-Dale Area.

f. Improve racial balance within the neighborhood.

g. Ensure visual ties within the Sylvan-Dale Area and links with the remainder of the City.

h. Ensure design and use compatibility of future development with existing development.

i. Implement park and open space plans.

j. Provide for a sense of neighborhood unity through design of public facilities.

k. Ensure compatibility of transportation facilities and land uses.

l. Improve transit options, pedestrian facilities and other options to automobile use.

TRADEOFFS

The goals and objectives are not necessarily mutually compatible, especially when neighborhood or personal goals are involved. Tradeoffs must be recognized and accepted if a balance of the goals is to be achieved. The Plan which follows strives to consider these tradeoffs and weigh their significance to achieve overall balance.

NEW DIRECTIONS

Since many of the goals with respect to open space, density and design, both neighborhood and City, do not necessarily follow past trends in the development of the area, a concerted effort will be needed to follow the new direction.

SECTION III: THE PLAN

The plan is divided into three general PARTS: Land Use, Circulation, and Environmental. Within these frameworks, it attempts to approach the issues of social concern, economics, and implementation. This Plan is not just a "physical" plan but rather a plan for the living environment of the Sylvan-Dale Area as a part of the City as a whole.

SECTION III-A: LAND USE

The Land Use Section deals quantitatively with areas of HOUSING, RECREATION-OPEN SPACE, COMMERCE, and EMPLOYMENT. It sets policy for the distribution, change, and balance of each of the components of land use. Greater emphasis on the qualitative aspects of these uses is covered under the environmental design section of this Plan.

The Plan makes several assumptions about the form of the area. First, it assumes minimal change or disruption of existing development. Second, it assumes that most of the land owned by the Mountain View Elementary School District will be sold, not retained for school purposes; and third, it assumes the area will reach full development within the next 10 years, with all available land occupied by an urban use, neither vacant nor in agriculture. The following map depicts the planned distribution of land uses.

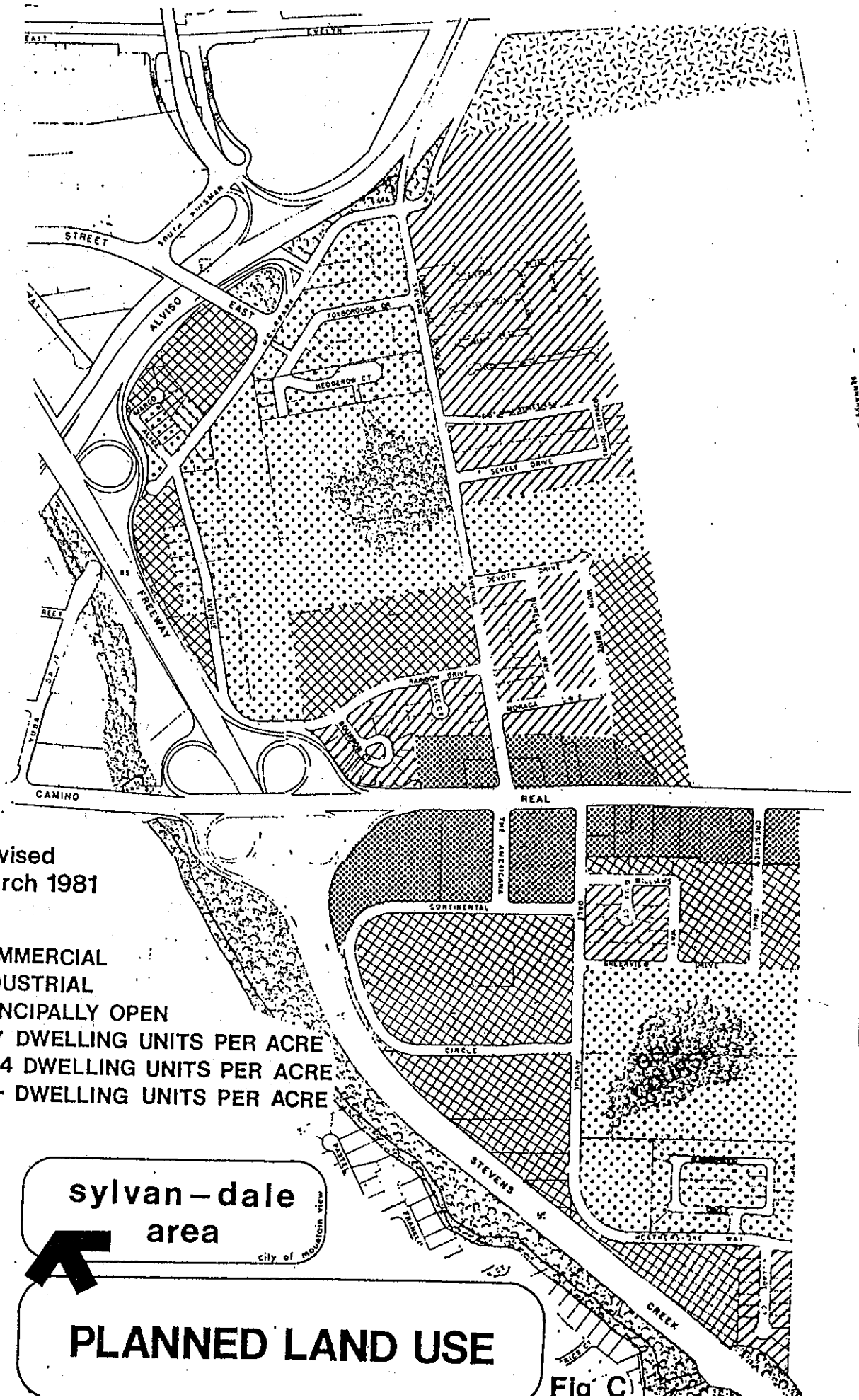

Revised
March 1981
COMMERCIAL
INDUSTRIAL
PRINCIPALLY OPEN
4-7 DWELLING UNITS PER ACRE
8-14 DWELLING UNITS PER ACRE
15+ DWELLING UNITS PER ACRE
sylvan-dale
area
city of mountain view
PLANNED LAND USE
EVELYN
STREET
ALVISO
EAST
FREEWAY
CAMINO
REAL
SEVELY DRIVE
CONTINENTAL
CIRCLE
STEVENS
CREEK

Fig C

HOUSING

Housing within the Sylvan-Dale Area is central to the remainder of the design. City-wide and neighborhood goals both indicate the need to modify past trends in residential development within the area to achieve the mixed residential area envisioned in the General Plan. Attendant to these changes will be modifications to the circulation patterns, public facilities, and overall design needed for the area.

The already high commitment to multiple-family, high-density living accommodations in the Sylvan-Dale Area means emphasis must be placed on new residential forms. Further, these forms must maximize potential for ownership, open space, family accommodations, and low- to moderate-income housing in order to meet the goals of the Plan.

To maximize these objectives means that selecting all low-density is not the answer to the land use plan. A true mixed residential area should not be created and a variety of housing types created.

Table VIII depicts the housing count anticipated under this plan; Table IX portrays anticipated population ranges.

TABLE VIII - APPROXIMATE NUMBER OF DWELLINGS BY TYPES**

HOUSING TYPE*	EXISTING		FUTURE ESTIMATE	
	No. of Units	% of Total	Est. No. of Units	% of Total
Single-family (Low Density)	51	2%	290	9.9%
2-4 family (Medium Density)	313	13%	380	13.6%
5+ family (High Density)	1,639	68%	1,830	62.7%
Mobile Homes (Low to Med.)	422	17%	420	14.4%
	2,425		2,920	

*With the introduction of Planned Unit Development, condominiums, common green development, etc., the classifications of density and ownership are intermixed.

**Amendment Nos. 5 and 6 (1980) added 170 units of housing to the Cherry Chase Golf Course land. The overall density is low (5.5± du/acre), but the five-story buildings will have many characteristics of higher-density, multiple-family housing. The above table and following text have not been revised to reflect these units.

TABLE IX - PROJECTED POPULATION RANGES

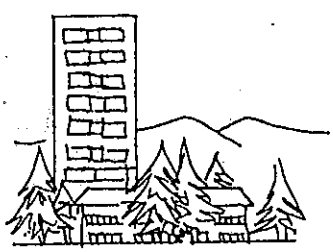
HOUSING TYPE	WITHIN EXISTING HOUSING LEVEL			WITHIN PROPOSED HOUSING LEVEL			
	A*	B*	%	A	%	B	%
Single Family (Low Density)	178	183	4	1,010	15.1	1,040	13.1
2-4 Family (Medium Density)	960	1,095	18	1,160	17.4	1,350	17.0
5+ family (High Density)	3,310	4,260	63	3,700	55.5	4,760	59.9
Mobile Homes (Low to Med.)	802	802	15	800	12.0	800	10.0
TOTALS	5,250	6,340	100	6,670	100.0%	7,950	100.0

Note: Projections assume family sizes will increase at least to City average.

*"A" refers to City-wide family size in 1968; "B" refers to the projected family size of the General Plan.

14.

High-Rise



Few new apartments similar to the low-rise existing are added under the Plan. However, up to 270 dwelling units of high-rise, limited to a maximum 13 stories may be added to The Americana site, just south of the Emporium site. In order to meet the objectives of the Plan, it is essential that the building be high-rise, not a continuation of the existing three-story design. Should the option not be taken, the plan requires retention of the existing open space recreation facilities. The site location, near the freeway and commercial uses, reinforces the high-rise concept. Use of a high-rise structure or structures should preserve a substantial amount of the land in open space, with no more than 25 percent of the land devoted to structures and automobile usage. 50

The high-rise will implement the goals of housing densities by introducing a new concept of living in the area, with accommodations that may serve senior citizens or persons of higher income in a unique setting. The ultimate density will depend on the nature of the occupancy and the resultant impact on the area, e.g., a higher number of units would be allowed for orientation to senior citizens rather than occupancy for families since smaller units and fewer cars could be expected.

The only other multiple-family areas in the Plan are on undeveloped lands off of Moorpark which are presently zoned R3*. Its relationship to surrounding development makes multiple-family development most logical to complete existing patterns.

High-Density

The proportion of multiple-family remains high (64 percent of the total number of units), a decrease of 4 percent over the existing proportion.

Family Structure

Existing multiple-family uses almost exclusively cater to singles or childless families. Under these circumstances and with present family sizes, the multiple-family population will stand at about 3,700 persons, of which 130 will likely be of elementary school age (K-6). However, should management policy on apartments change, the school-age population will rise significantly. If multiple-family size reaches the size assumed by the General Plan (Range B), 4,760 persons would be housed in the multiple-family units, including 370 elementary (K-6) school age.

Since this is a midrange, 10-year plan, it is not expected that an explosive change in school-age children in multiple-family residences will occur. The possibility should be remembered, however, particularly with respect to future school needs and possible disposal of school-owned land.

As an overall City policy, consideration should be given to appraisal and, if necessary, regulation of some rental policy. Better understanding of whether or not an apartment was to be for families or not will have a significant impact on public facilities such as the amount and type of recreation space and the need for schools. Moreover, housing for families should be designed for the use of children. Structural design should be such to withstand hard use; site design should be oriented to the use by children. Both aspects are important to achievement of a quality living environment.

Medium Density

Medium density consists of density of 8 to 14 units per acre. Provisions for approximately 380 units are made in the Plan. Thus, 13 percent of the housing in the Sylvan-Dale Area will house 1,160 people or 17.0 percent of the population, including approximately 90 K-6 age children.

The 4.2-acre parcel abutting Highway 85 along Rainbow Drive shall be devoted to medium-density, planned unit residential use. The following criteria shall apply to development:

1. The density shall be limited to 10 dwelling units per <u>net</u> acre (limiting total number to 36 units).

2. A decorative masonry sound wall and heavy buffer landscaping will be provided adjacent to Highway 85, El Camino Real and existing commercial uses. The wall must be set back adequately along El Camino Real to provide property owner-maintained landscaping between the sidewalk and wall.

3. Rainbow Drive will provide the sole means of vehicular access. Pedestrian access to El Camino Real shall be encouraged.

4. Home ownership and moderate-income options shall be encouraged.

Mobile Home Parks

Mobile home parks fall within the medium density range, with an average of approximately 10 dwelling units per acre, about the same density as duplexes. The new (1971) ordinance allows approximately 7 dwelling units per acre, a low-density development range.

The two parks on Sylvan are relatively new and likely to remain for a considerable time. The Moorpark Mobile Home Park is one of the oldest in the City and market conditions might lead to changes in its nature.

The policy of the Plan shall be to retain the existing mobile home parks and encourage their continued upkeep to ensure they remain as a source of quality, moderately priced housing. The mobile home park is a primary source of housing for many senior citizens. The City shall encourage modernization and redevelopment of existing R2M lands at new density and development standards. Should a major change in use be required, only low-density residential zoning shall be considered.

A policy should be pursued to ensure the integration of the parks into the infrastructure of the emerging neighborhood, rather than total isolation. One mechanism to this end will be consideration of extending linkage from Alice Avenue to the proposed neighborhood park at the time the park and surrounding residential lands are developed. The linkage may consist of street or merely pedestrian and bicycle paths, to be decided at the appropriate time. Vehicular access will heighten emergency and police access as well as tie the westerly portion of the area to the center of the neighborhood.

Low Density

The largest number of dwelling unit types to be added within the area is low-density, single-family accommodations. While they would only make up 10 percent of the final housing count, they account for over 55 percent of the new units to be added within the area. It is assumed that the low-density residential use will provide one of the best means to implement the goals of ownership, increased civic responsibility, accommodations for families, and residential identity. However, there is a question as to its ability to provide housing for moderate-income families or to maximize housing opportunities.

The low-density range within the Sylvan-Dale Area extends from 4 to 8 dwelling units per gross acre of land. This may include several residential zones. As an example, on raw land, the following low-density zones would have the following density characteristics if 25 percent of the area is devoted to streets.

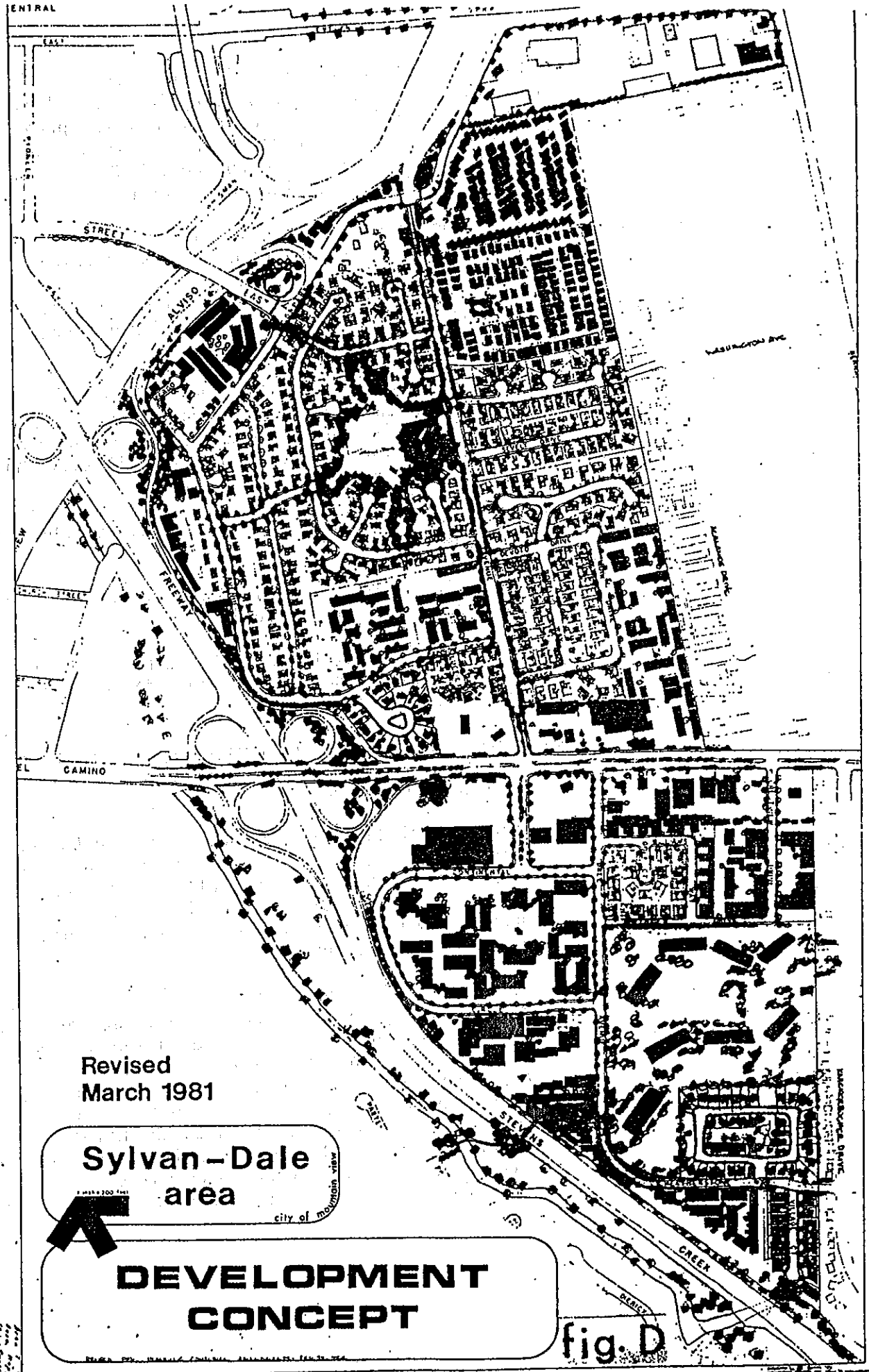
Zone	Maximum Density
R1-7L	4.7 du/acre
R1	5.4 du/acre
R2-10L	6.5 du/acre
R2-9L	7.3 du/acre

An estimated 1,010 persons will be housed in low-density residences, 160 of elementary school age. The Plan assumes that as the low-density neighborhood grows, the average family sizes will also grow to at least the City average. The family size may be expected to surpass the City median with a higher-than-average number of elementary school age children as is typical in new residential areas.

Approximately 80 percent of the proposed "low-density" range of development (less than 8 du per acre) is located north of El Camino Real, where vacant land is available. The proposed zoning ranges from R1-8L to R2, with larger minimum lot sizes to encourage a PUD approach and a higher level of ownership than is typical of a duplex subdivision. The sd (special design) and P zones are also recommended where environmental factors such as noise or relationship to adjoining uses call for special design to achieve community goals.

The remaining approximately 20 percent of the low-density residential dwelling units shall be located along the north side of Heatherstone Way, where under the P zone a maximum of 44 dwelling units may be constructed under the provisions of this Precise Plan. Special attention will be given in review of design proposals to the relationship between this approximately 10-acre site, its structures and surrounding properties. The proximity of the existing open use (the golf course) underscore the need for special design care in order to assure that visual and physical linkages are provided where possible. The architecture of the structures on this site must take into account the relationship they will have with surrounding developed and underdeveloped prooperties and to insure an effective integration into the neighborhood. Sufficient circulation and parking provisions will be made to guarantee that no parking problems will develop along Heatherstone Way. Conventional "R1" subdivision would have difficulty meeting these design objectives, whereas a planned development incorporating a mixture of structure types, clustered living areas, open space and direct linkages to the adjacent open areas would more easily meet the objectives.

A unique mixed-residential and open space use of overall low density may be developed along Dale Avenue on the site of the Cherry Chase Golf Course. The development shall include the


CENTRAL
STREET
ALVISO
EAST
FREEWAY
EL CAMINO
WASHINGTON AVE
STEVENS
CREEK
Revised
March 1981
Sylvan-Dale
area
city of mountain view
DEVELOPMENT
CONCEPT
fig. D

continued use of an improved nine-hole public golf course which shall also serve as a setting for a development of up to 170 residential units. The design of the project will take into account not only the need for additional housing in the community, but also the need to protect the recreation and open space values of the existing golf course.

The following criteria shall be followed in carrying out any such combination residential/golf course development:

1. A well-designed, functional nine-hole golf course open to the public shall be retained and integrated with the residential use;

2. The development shall include all of the approximately 30 acres shown on attached Figure D (Development Concept) and shall include the coordinated use of the recreational clubhouse facilities which lie within the City of Sunnyvale along Knickerbocker Drive.

3. Not more than 170 residential units may be developed on the site.

4. Buildings shall be limited to a height from grade of not more than three stories.

5. Fifty percent of the units must be at least three-bedroom and 80 percent of the units must have at least two bedrooms.

6. The development shall include appropriate on-site recreational facilities to serve diverse ages of project residents.

7. The site shall ensure visual access to the golf course from the street. Setbacks, sidewalk and streetscape design along Dale Avenue shall respect the need to foster the sense of features including, at least, the use of meandering walks, provision of benches and clustering of trees.

8. Any development applicant shall be fully responsible for providing to the complete satisfaction of the City Council, at the time of consideration of a specific development plan, private means, conditions and assurances for future retention of the golf course and, in the event the golf course should cease, that title to the golf course area goes to the Homeowner's Association, which shall be responsible for maintaining that area as private open space unless a use change is approved as hereinafter provided. Said assurances

shall include a provision in the CC & R's running in favor of the Homeowner's Association that a change in use of the golf course from a golf course or open space shall not take place without a favorable vote of at least 80 percent of the members of the Homeowner's Association eligible to vote, without the exercise of cumulative or weighted voting. Said assurances shall also include a scenic easement in favor of the Homeowner's Association guaranteeing retention of the open space (although incidental accessory structures may occur with prior City approval), unless a favorable vote of at least 80 percent of the members of the Homeowner's Association eligible to vote is obtained, without the use of cumulative or weighted voting. The wording of this change in use provision and the scenic easement shall be subject to approval by the City Attorney. Any exercise by the Homeowner's Association of its right to change said provisions cannot be exercised without an 80 percent vote in the same manner as above noted; nor may the Homeowner's Association approve any change in said provisions without the prior consent of the City Council.

9. The design concept generally depicted in Figure D shall be carried through and implemented with the coordinated input of the highest levels in architecture and golf course design talent in order to ensure quality, compatible development of the golf course, landscaping and housing.

HOUSING CONDITION

There are few problems at present with respect to adequacy of existing housing since virtually all the units are new. Consideration must be given in City ordinances to insure that existing development continues to be maintained in a standard condition.

OWNERSHIP

The Plan provides an opportunity for property ownership, primarily by increasing the number of low-density residential dwelling units.

HOUSING COSTS

The Sylvan-Dale Area Plan encourages the addition to the area of housing oriented to a wide range of income families--low, moderate, and high. Virtually all of the existing (1973) housing is oriented to moderate- to high-income groups. The bulk of the proposed housing will also tend to be in the same price range irrespective of density.

Providing low- and moderate-income housing is a City-wide rather than a local neighborhood goal. Meetings held within the area did not yield a significant amount of positive input with respect to implementing this City-wide goal. To the contrary, many people within the area expressed reluctance to housing of any type which would have a value less than the

value of their own property. In order to alleviate some of the fears associated with low- and moderate-income housing, it is essential that review procedures be maintained for review of the physical design of such housing, particularly as it relates to surrounding properties.

It is apparent within today's market that the best way to add new housing for low- and moderate-income families is through housing programs which either directly subsidize the occupant or provide incentives to the market. Past Federal programs directed to this end are no longer available for low- or moderate-income families.

Local housing programs must be relied on in the future to achieve any level of low- and moderate-income housing. The policy of this Plan is for the City to pursue the necessary mechanisms which will bring about City-wide housing goals of the General Plan and apply these throughout the community.

HOUSING FOR SENIOR CITIZENS

The nature of existing developments with a high proportion of both apartments and mobile homes within the area provides housing for older citizens within the community. Since one of the major problems of housing for the elderly is not just availability, but also housing cost, consideration should be given under this category, as well as low- and moderate-income housing, for specific housing for senior citizens. The proximity of this area to many neighborhood service facilities may make housing for senior citizens desirable within the area. However, isolation from other community activities, recreation facilities, and medical facilities raises some question as to the desirability of much more senior citizen housing within the Sylvan-Dale Area. It would seem better to locate such facilities in the central part of the City where facilities and transportation are guaranteed.

SPECIAL LIVING ACCOMMODATIONS

There is an increasing need within the community for group facilities for certain groups of citizens. Each neighborhood within the City should be responsible for providing such accommodations. Those particularly in need are the very old who need some level of care, the physically handicapped, and the mentally ill or retarded. State programs have been set up to foster such living accommodations. It is incumbent upon local jurisdictions to prepare or plan for such facilities and provide a rational means for distribution within the community.

At present, approximately .2 percent of the City population is utilizing residential care facilities, mostly elderly citizens. It must be assumed that this number will increase during the Plan period in light of greater emphasis on residential care and because of an increasing older population. At present, 10 "beds" are provided in the Sylvan-Dale Area, equivalent to the City-wide ration. This Plan recommends that no more than .5 percent of the population be composed of

residential care facilities until such time as a more complete allocation model is developed City-wide. Design of residential care accommodations should ensure that the intensity of use is comparable with that of adjoining uses.

COMMERCIAL

Undeveloped Commercial

For the most part, the commercial land area within the Sylvan-Dale Area is already defined and relatively static. Approximately 3.5 acres of commercial land along El Camino remain to be developed. Uses should be introduced, where possible, that would provide direct neighborhood goods and services, such as a clothing shop, produce outlet or restaurant.

The structure which houses the 7-Eleven store on Moorpark and Sylvan, approved prior to incorporation, is new and will last beyond the term of this Plan. The store at this location provides limited services to residents but has little advantage over a location on El Camino. The plan recommends a P zone on this property to ensure that future uses in the building provide neighborhood commercial uses. The site is poorly designed and ineffectively landscaped. The owners should be encouraged to update the site and improve maintenance.

The Plan designates the lands along Evelyn Avenue from Bernardo to Moorpark for light industrial uses. Approximately 9 acres of this industrial area are already developed and approximately 6.8 acres remain for development. Light industrial use, rather than expansion of the general industrial uses, would help insure a better relationship between the industrial lands and abutting residential lands to the south. Attention must be given to physical buffering between the uses.

In order to insure the proper transition from the general industrial uses to limited industrial uses, and to insure the protection of already developed land, consideration should be given to application of the Planned Community District, where appropriate, with general application of the ML District modified by the following provisions:

1. In addition to the ML uses, the following shall be considered principal permitted uses within the subject areas:

    a. Wholesale uses.

    b. Warehousing and indoor storage of nonhazardous materials.

    c. Accessory uses customarily appurtenant to a permitted use.

    d. Properly screened or fenced outside storage amounting to no more than 10 percent of the covered area.

2. All existing occupancies shall be considered permitted uses. Changes in occupancies that are to occupancies allowed above or that do not increase the intensity of existing use shall be subject to the administrative review of the Zoning Administrator following normal review procedures for the ML District and shall not require a PC permit.

3. Existing structures may be reconstructed at MM development standards in the event of major damage.

4. Specific Development Criteria:

   a. A minimum 5' landscaped buffer-screen zone along the rear property line, unless building wall is located at rear property line; in such case, rear elevation to be treated in an architecturally aesthetic manner.

   b. No use permitted that would emit excessive noise or odor pollution or lighting obtrusive to adjacent uses.

5. Signing, outdoor storage, parking changes and minor structural changes shall be subject to the administrative approval of the Zoning Administrator. Other changes in land use and new construction and structural additions not listed above shall require a PC permit from the City Council after recommendation by the Zoning Administrator.

SCHOOLS

Two factors will determine the need for construction of a school in the Sylvan-Dale Area. One is the district-wide needs; another is neighborhood development patterns. Mountain View Elementary School District is responsible for the final decision.

Future School Needs

At present, there is excess classroom capacity within the District, although busing is required to balance the student population from one neighborhood to another. The national trend in declining birth rates and family sizes will continue to affect the need for an elementary school. However, within the City of Mountain View, this general decline in school-age population may be balanced or surpassed by other trends which are emerging.

The first, and perhaps the most important trend, is an increased emphasis to provide more low-density residential areas. Such an emphasis will mean a higher percentage of family living accommodations will be built over the next several years.

The second City-wide trend is the use of multiple-family lands for condominium and common green development. Although, at the present time, these accommodations do not provide a particularly high number of school-age children, that situation is expected to change.

The third trend is the possible change in the nature of apartment rentals. In the past, most of the City apartments have restricted families with children. There is some evidence that this policy is changing, particularly in older units. In addition, new units are being constructed with emphasis on family accommodations. In the latter case, the family size and school-age population is equivalent to that found in single-family residential areas throughout the City.

ELEMENTARY SCHOOL

An elementary school in the Dale section and an elementary as well as a junior high school in the Sylvan section had been previously planned in the Sylvan-Dale Area. Forty-three acres of land are owned by the District for these schools. Under this Plan, it is projected that only one elementary school will be constructed because of school-age growth trends. Assuming up to 15 acres of the existing District land are held for a school park, the remaining 28 acres will be added to residential use. With low- and medium-density development, an elementary school-age population within the Sylvan-Dale Area can be expected to approach 370 to 670 (as indicated in Table X) within the next 10 years. Optimum enrollment for an elementary school is 500 students. In the future, beyond the 10-year period of this Plan, it can be expected that conversions of existing mobile home parks along Sylvan may also take place, further increasing the family accommodations within the Sylvan-Dale Area.

The Plan does not stress the need for an elementary school within the period of this Plan. However, it sets as policy that adequate acreage be maintained within the area for an elementary school facility based on expected future demands. Approximately 10 acres of land in the Sylvan section are set aside for public purposes. Development of the first phase of a neighborhood park would be followed by development of a school in conjunction with expansion of the park. The centrally located public lands would be served by pedestrian links to surrounding residential uses.

OPEN-SPACE LAND

Of the 340 acres of land within the Sylvan-Dale Area, approximately 40 are designated for public and quasi-public open uses. This acreage includes approximately 25 acres for the golf course in the Dale triangle which may be redeveloped in conjunction with housing. The acreage does not include the extensive amount of private open and recreational areas in the Sylvan-Dale Area. The Sylvan Park located north of El Camino is designed to serve the bulk of the residential area. Implementation of a pedestrian/bicycle overpass across Route 85 will provide improved access from the Dale area to Cuesta

Park, Cooper Park, Huff School and the Stevens Creek Park Chain.

Access to existing schools in other parts of the community should also be improved. The Plan recommends improvement of pedestrian and bicycle facilities along Dana Street to Landels School and, secondly, the construction of an overpass in the Dale area connecting into Huff and Cooper Schools in the Grant Area. These measures will improve access for school purposes as well as serve to generally link the Sylvan-Dale Area with the remainder of the community and its public facilities.

Besides the educational purposes, the school can serve to define the character of the neighborhood. Greater family emphasis can be expected. Greater life and activity within the neighborhood will be experienced.

The school will also provide meeting facilities for community activities. In conjunction with the park, the public expenditures of both the City and School District can be extended. The design of the school-park should incorporate both District and City needs. Consideration should be given to incorporating preschool facilities either in the school or on adjacent lands.

The Plan recommends development of a public facilities zone for application to the School District and other publicly-owned lands.

TABLE X - ELEMENTARY SCHOOL AGE POPULATION PROJECTIONS

	No. of Units	K-6 Factors		K-6 Population	
		A	B	A	B
Low Density	290	.56	.6	162	174
Medium Density	380	.24	.4	91	152
High Density	1,830	.07	.2	128	366

--Assumes continued mobile home park policy of excluding families with children.

"A" refers to City-wide K-6 per dwelling unit, 1968.
"B" refers to projected K-6 population projected by the General Plan.

Stevens Creek Park Chain

The Stevens Creek Park Chain, a regional open space facility, is linked to the Dale Area via the proposed overpass and a possible access point at the terminus of Village Court. The creek is to be left in a relatively natural setting, not developed with active recreational facilities.

Because of the long-term nature of the implementation program for the Stevens Creek Park Chain, the Plan does not provide for means of access from Village Court at the present time. A small area at the end of Village Court where development is restricted because of utility easements could serve as the future point of entry. In the interim, it is recommended that the currently vacant area be landscaped as an accent point within the neighborhood, preferably via a volunteer neighborhood project. (Figure H)

Golf Course

The 25-acre, 9-hole Cherry Chase Golf Course not only serves the neighborhood and community, but is fully compatible with surrounding residential and public open space uses. Further, the open use serves to balance uses throughout the community providing a level of private as well as public recreational facilities. The Sylvan-Dale Area Plan is based upon the continued use of the Cherry Chase Golf Course as a quasi-public open space facility, even though developed in conjunction with integrated housing.

Sylvan Area Park

The characteristics of the northern half of the Sylvan-Dale Area are such that a more extensive neighborhood park is required. The park will provide essential recreation facilities, including ballfields, picnic areas, and tot lots which will further enhance the low-density residential character of the area. The park will provide essential community identity now lacking within the area.

Since the Mountain View Elementary School District questions the need for either of these schools within the area, this Plan recommends maintenance of 09 acres of publicly-held land for a combination park and school purpose. Should the elementary school-age population projections not bear out in the next 10-year period as the area matures, the total acreage should be utilized for park purposes. The Plan recommends high priority be given to the implementation of Phase I of the park since it will serve as a central focal point around which residential development will take place. The first phase should begin within the first five years of this Plan and include substantial planting of mature trees and other natural vegetation.

State Lands

An additional open space area consisting of three parcels (3.7 acres) of State-owned lands along Moorpark Way should be added to the neighborhood. These lands are now vacant remnants resulting from freeway construction. While these lands do not provide an opportunity for high-intensity recreation use because of their separation from the bulk of the area, they may serve the function of providing a special purpose recreation area such as community gardens. Figure 1

depicts the possible design for these lands. An estimated $35,000 may be available from the State Highway Department under provisions of the Marler-Johnson Highway Act to implement this use.

A second area which has potential as an open space area and possible tot lot is located at the intersection of Alice and Moorpark, adjacent to the freeway on-ramp. This triangular parcel of land is undevelopable unless combined with adjacent lands to the south. The Plan places as a low-priority item the option, later in the planning period, to purchase and develop a small tot lot.

Several areas may serve as visual accent points and are more thoroughly covered in the design section of the Plan.

IMPLEMENTATION OF LAND USE POLICY

The City has three principal ways of ensuring that the land use policies of this plan are implemented: zoning, development review, and capital improvement planning.

Zoning

Zoning is perhaps the most direct and readily applied means available. The list of rezoning actions which follows in Appendix B details each of the City-initiated rezonings which is needed to implement this Plan.

Site Plan and Architectural Review

Development control includes design review by all City departments. This Plan provides guidance to those making the review, framing the specific project with broader policy. Also included in the development controls are ordinance standards. The Plan recommends modification of the residential sections of the Zoning Ordinance to (1) require closer architectural review of the design of R1 and R2 subdivisions; (2) include provisions for development and density bonusing or residential zones for low- and moderate-income housing; (3) provide for a planned unit development overlay, and (4) vary the density in the R3P zone.

Capital Improvement Planning

The third technique is capital improvement planning which applies to both the City and the Mountain View Elementary School District. Timing of development of park and school facilities, emphasis on transit, and allocation of funds for pedestrian facilities will all shape the form of the neighborhood. The plan specifies areas of emphasis and priority. The highest priority items are purchase of the Dale Park overpass expansion area, funding for the first phase of Sylvan Park and funding of housing programs.

Stevens Creek Park Chain

The Stevens Creek Park Chain, a regional open space facility, is linked to the Dale Area via the proposed overpass and a possible access point at the terminus of Village Court. The creek is to be left in a relatively natural setting, not developed with active recreational facilities.

Because of the long-term nature of the implementation program for the Stevens Creek Park Chain, the Plan does not provide for means of access from Village Court at the present time. A small area at the end of Village Court where development is restricted because of utility easements could serve as the future point of entry. In the interim, it is recommended that the currently vacant area be landscaped as an accent point within the neighborhood, preferably via a volunteer neighborhood project. (Figure H)

Golf Course

The 25-acre, 9-hole Cherry Chase Golf Course not only serves the neighborhood and community, but is fully compatible with surrounding residential and public open space uses. Further, the open use serves to balance uses throughout the community providing a level of private as well as public recreational facilities. The Sylvan-Dale Area Plan is based upon the continued use of the Cherry Chase Golf Course as a quasi-public open space facility, even though developed in conjunction with integrated housing.

Sylvan Area Park

The characteristics of the northern half of the Sylvan-Dale Area are such that a more extensive neighborhood park is required. The park will provide essential recreation facilities, including ballfields, picnic areas, and tot lots which will further enhance the low-density residential character of the area. The park will provide essential community identity now lacking within the area.

Since the Mountain View Elementary School District questions the need for either of these schools within the area, this Plan recommends maintenance of 15 acres of publicly-held land for a combination park and school purpose. Should the elementary school-age population projections not bear out in the next 10-year period as the area matures, the total acreage should be utilized for park purposes. The Plan recommends high priority be given to the implementation of Phase I of the park since it will serve as a central focal point around which residential development will take place. The first phase should begin within the first five years of this Plan and include substantial planting of mature trees and other natural vegetation.

State Lands

An additional open space area consisting of three parcels (3.7 acres) of State-owned lands along Moorpark Way should be added to the neighborhood. These lands are now vacant remnants resulting from freeway construction. While these lands do not provide an opportunity for high-intensity recreation use because of their separation from the bulk of the area, they may serve the function of providing a special purpose recreation area such as community gardens. Figure 1

depicts the possible design for these lands. An estimated $35,000 may be available from the State Highway Department under provisions of the Marler-Johnson Highway Act to implement this use.

A second area which has potential as an open space area and possible tot lot is located at the intersection of Alice and Moorpark, adjacent to the freeway on-ramp. This triangular parcel of land is undevelopable unless combined with adjacent lands to the south. The Plan places as a low-priority item the option, later in the planning period, to purchase and develop a small tot lot.

Several areas may serve as visual accent points and are more thoroughly covered in the design section of the Plan.

IMPLEMENTATION OF LAND USE POLICY

The City has three principal ways of ensuring that the land use policies of this plan are implemented: zoning, development review, and capital improvement planning.

Zoning

Zoning is perhaps the most direct and readily applied means available. The list of rezoning actions which follows in Appendix B details each of the City-initiated rezonings which is needed to implement this Plan.

Site Plan and Architectural Review

Development control includes design review by all City departments. This Plan provides guidance to those making the review, framing the specific project with broader policy. Also included in the development controls are ordinance standards. The Plan recommends modification of the residential sections of the Zoning Ordinance to (1) require closer architectural review of the design of R1 and R2 subdivisions; (2) include provisions for development and density bonusing or residential zones for low- and moderate-income housing; (3) provide for a planned unit development overlay, and (4) vary the density in the R3P zone.

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AUTOMOBILE FACILITIES

The automobile and its attendant facilities are still the most important element in establishing the basic internal circulation pattern. A minimum "C" level of service will be sought, but a lower level will be accepted where improved facilities will jeopardize the adjoining land uses.

In the area north of El Camino, Sylvan Avenue, Moorpark Way, Alice Avenue, and Rainbow Drive form an internal street loop. The heaviest volumes will continue to take place on Sylvan Avenue and Moorpark Way. South of El Camino, The Americana, portions of Continental Circle, Dale Avenue, and Heatherstone Way provide a central traffic carrier. All other streets within the area either serve as connecting points to arterials or are streets providing primary access for residential development.

Dana Street

A change from existing policy is the abandonment of the plan line for the extension of Dana Street as a 90', four-lane arterial from Moorpark to Sylvan and thence to Sunnyvale. The reasons for this change of policy concern the goals to optimize the residential potential of the area and improve the living environment. A major through street will sever the residential area, visually separating the neighborhood and dividing public facilities from much of the residential area. Additional traffic would be diverted through the area, adding noise and pollution. The new policy will help alternate, periphery routes along Evelyn Avenue and El Camino for access to the freeway.

The policy recognizes that there will be some delay to motorists, particularly from outside the neighborhood. The policy will, however, have little effect on the City-wide auto circulation system.

STREET IMPROVEMENTS

Sylvan Avenue

Some stretches of the primary streets need improvement. Most in need is Sylvan Avenue, with approximately 50 percent of its length in substandard condition. Figure J depicts a conceptual plan for its development which ties the street into the residential neighborhood, functionally and visually. A detailed geometric plan should be developed and approved following adoption of this Plan. Improvements for the street should be underway by 1977.

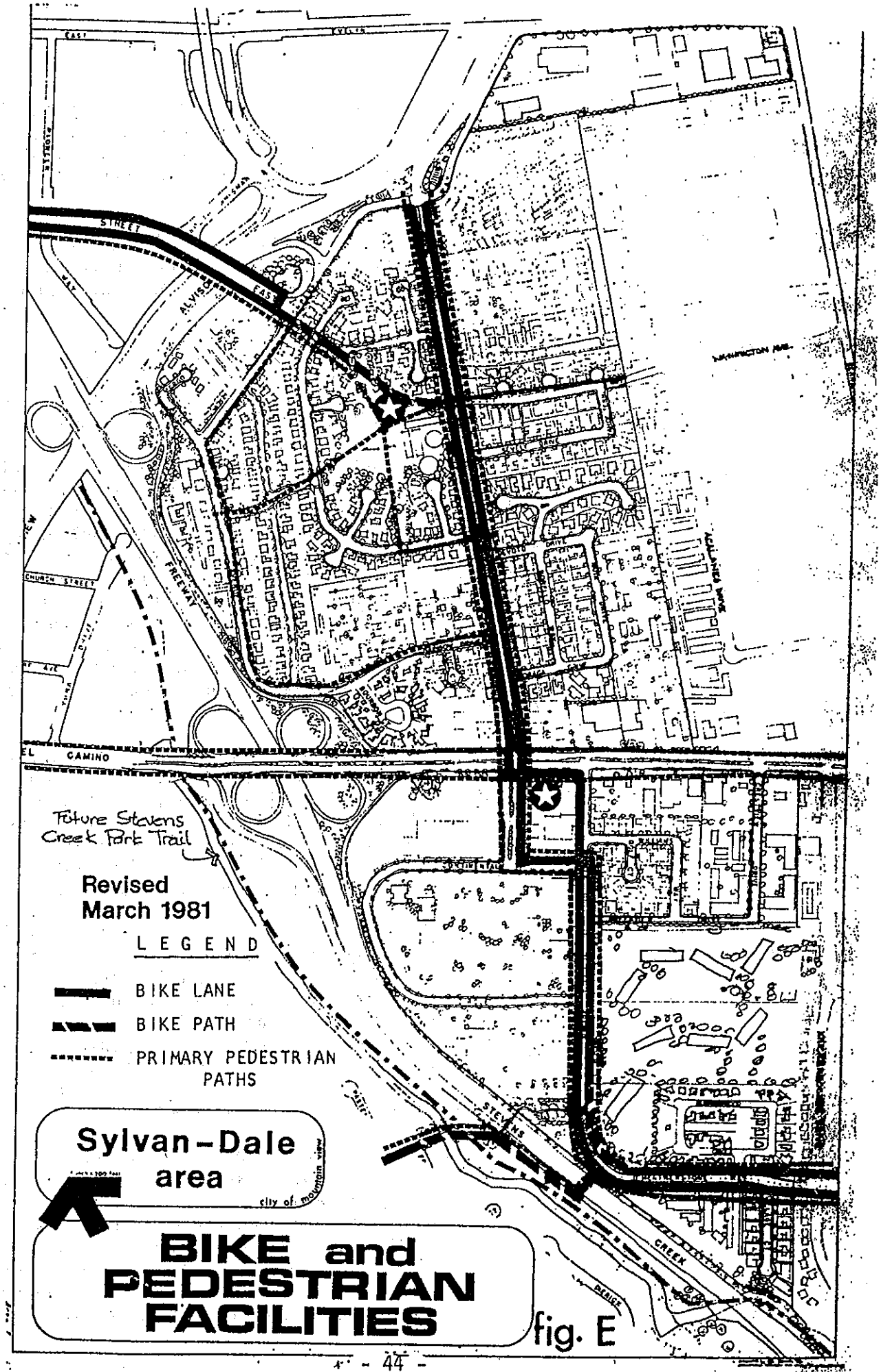
Dana Street (Sylvan to Washington)

The completion of Dana Street between Washington Avenue in Sunnyvale and Sylvan Avenue should proceed immediately as required by conditions of approval of the mobile home park to the north. The street shall be terminated in a cul-de-sac at the City limit line. Provision for pedestrian and bicycle passage shall be maintained. The street name should be changed to avoid confusion.

SECTION III-B: CIRCULATION

The Circulation component of the Plan complements the land use policy. The system is designed to avoid conflicts between traffic flows and adjacent land uses. An increased emphasis is placed on pedestrian and bicycle circulation and on transit. It is the design objective that renewed emphasis on non-auto-oriented facilities will provide a better living environment within the area.

Alice Avenue	Alice Avenue from Moorpark to Rainbow is currently a substandard street in that it has no sidewalks, curbs, or gutters. The road surface is good, however, and in light of other requirements in the area, the street has a low priority for further improvements.
Dale Avenue	Minor modifications to the street design are recommended for Dale near Heatherstone in conjunction with construction of the park. The modifications are aimed at improving pedestrian access and slowing traffic.
Greenview Drive	Figure K, Exhibit B depicts a cross section and plan for this street which lies north of the golf course. The "special" street design will serve to continue the compatible relationship of the residential and golf course uses.
NEW STREETS	Additional streets will be needed within the Sylvan-Dale Area to serve the presently undeveloped lands lying east of Sylvan Avenue. Figure D shows a conceptual street pattern which discourages through traffic and maintains continuity between properties. Although modification and refinement of these street patterns is expected, particularly in conjunction with a PUD, it is essential that the street pattern in the vicinity of the terminus of Dana Street be designed in such a way as to defer traffic cutting from Dana Street to Sylvan Avenue via primarily residential streets. Also required are two points of vehicular access from Sylvan Avenue to the area west of Sylvan. It is also desirable that a connection be made from Alice to the new school-park area when developed. It may be necessary to bisect the existing mobile home park with either a full street or pedestrian and bicycle linkages. These special design features will serve to direct vehicular movement, to provide facilities for bicycles and pedestrians, and to enhance the relationship of the streets to the adjacent uses.
PEDESTRIAN AND BICYCLE FACILITIES	Figure E depicts the location of pedestrian and bicycle facilities within the Sylvan-Dale Area. Most of the facilities are designed into the basic street pattern. That is, traditional sidewalks and bicycle lanes are used but with greater emphasis based upon the quality of these facilities. Use of meandering sidewalks, corner crossing peninsulas, landscaping, and street furniture enhance the relationship of the paths to travel lanes and adjacent uses. The facilities will also enhance movement for the handicapped.
Parks	Specially designed pedestrian facilities are noted in the vicinity of the two public park areas. In the Sylvan Area, a pedstrian-bicycle path is maintained from Sylvan Avenue to Moorpark Way, running through the park and adjacent subdivisions. Sidewalk areas leading to the park for some distance on either side are intended to give the feeling of entering into a park setting. A meandering path is proposed for the length of Dale Avenue extending from Greenview along the


Future Stevens Creek Park Trail
Revised March 1981
LEGEND
BIKE LANE
BIKE PATH
PRIMARY PEDESTRIAN PATHS
Sylvan-Dale area
city of mountain view
BIKE and PEDESTRIAN FACILITIES
fig. E
EL CAMINO
FREEWAY
STEVENS
CREEK
CHURCH STREET
ALVISO
EAST
STREET

front of the golf course. The path gives a feeling of walking in a park setting and takes advantage of the surrounding open space areas.

Overpass

The Plan proposes construction of a pedestrian-bicycle overpass to bridge the Stevens Creek Freeway from the vicinity of Dale Avenue to the Stevens Creek Park Chain and thence to Franklin Avenue in the Grant Area. This overpass would tie together public open spaces on either side of the freeway and open up to residents of the Sylvan-Dale Area public facilities, including two elementary schools, a high school, Cuesta Park and the El Camino Hospital. Grades, structure, and open spaces at either end of the facility should be designed to encourage bicycle and pedestrian use.

The preliminary estimate for the cost of the structure is $167,600. A sidewalk is provided on the Dana Street freeway bridge leading to the central City. Bike lanes should be added to Dana Street.

No pedestrian overpass is recommended at this time to link the areas on either side of El Camino. The at-grade signalized intersection includes pedestrian movements in the signal phasing.

TRANSIT

Dial-A-Ride

The Plan assumes major emphasis on demand-responsive (Dial-a-ride) local service which provides direct door-to-door service for local travel. A fixed route system along El Camino and Evelyn will provide County-wide transit. Both are in accord with recent County-wide plans.

Street design along El Camino and Evelyn should provide furniture for the comfort of transit users. A phone station for Dial-a-ride shall be provided in the park areas since they will be important destinations.

PRT

The future concept of fixed rail personal rapid transit (PRT) can serve the area easily. Utilizing existing right-of-way, lines can traverse Dale, El Camino, Sylvan, Evelyn or freeway right-of-way. Easements for pedestrian movements can also support PRT lines. Service within a 1/4 mile walking distance is established as the preliminary goal.

IMPLEMENTATION

For the most part, implementation of recommended automobile, pedestrian, and bicycle facilities will take place as development of remaining vacant land is undertaken. Subdivision planning and development approval shall include provisions for special pedestrian-bicycle easements and construction and facilities indicated in Figures E through K.

Capital Improvements

Both Sylvan Avenue and the pedestrian overpass on Dale Avenue are considered high-priority items and should be included within the Capital Development Schedule for the next five years. The Plan recommends minimal reconstruction of existing developed sidewalk or street areas, giving them a low priority.

Precise Street Plans	Precise geometric plans will be needed for Sylvan and Moorpark (north of Dana Street and Evelyn Avenue). These plans shall follow the guidance of the concepts presented in this Plan, especially the design section. The General Plan as it relates to the Dana Street extension will need to be modified, deleting the street extension. The City street ordinance should also be amended in accord with the Dana Street policy and residential street standards policy.

SECTION III-C: ENVIRONMENTAL DESIGN

The past two sections of the Plan, Land Use and Circulation, have laid the basic design patterns of urban activity for the Sylvan-Dale Area. This section, Environmental Design, will seek to coordinate the urban activities in greater detail in order to ensure their compatibility. The section will set forth means to improve the human environment by guiding public and private development, by setting forth an understandable urban form, by minimizing negative aspects of the area, and by creating a desirable human element.

There is no gimmick theme or rigid style suggested by this section; nor does the section address itself only to the visual environment or aesthetics. Rather, means are provided for improving the existing environment, preserving elements of the natural environment, ensuring new development follows a logical pattern, and merging all these elements into a neighborhood which minimizes stress and raises the spirits of its inhabitants. Figure F is a map of the Sylvan-Dale Area which depicts general areas of design emphasis aimed at achieving these ends. Subsequent maps and diagrams which follow will build upon these basic ideas.

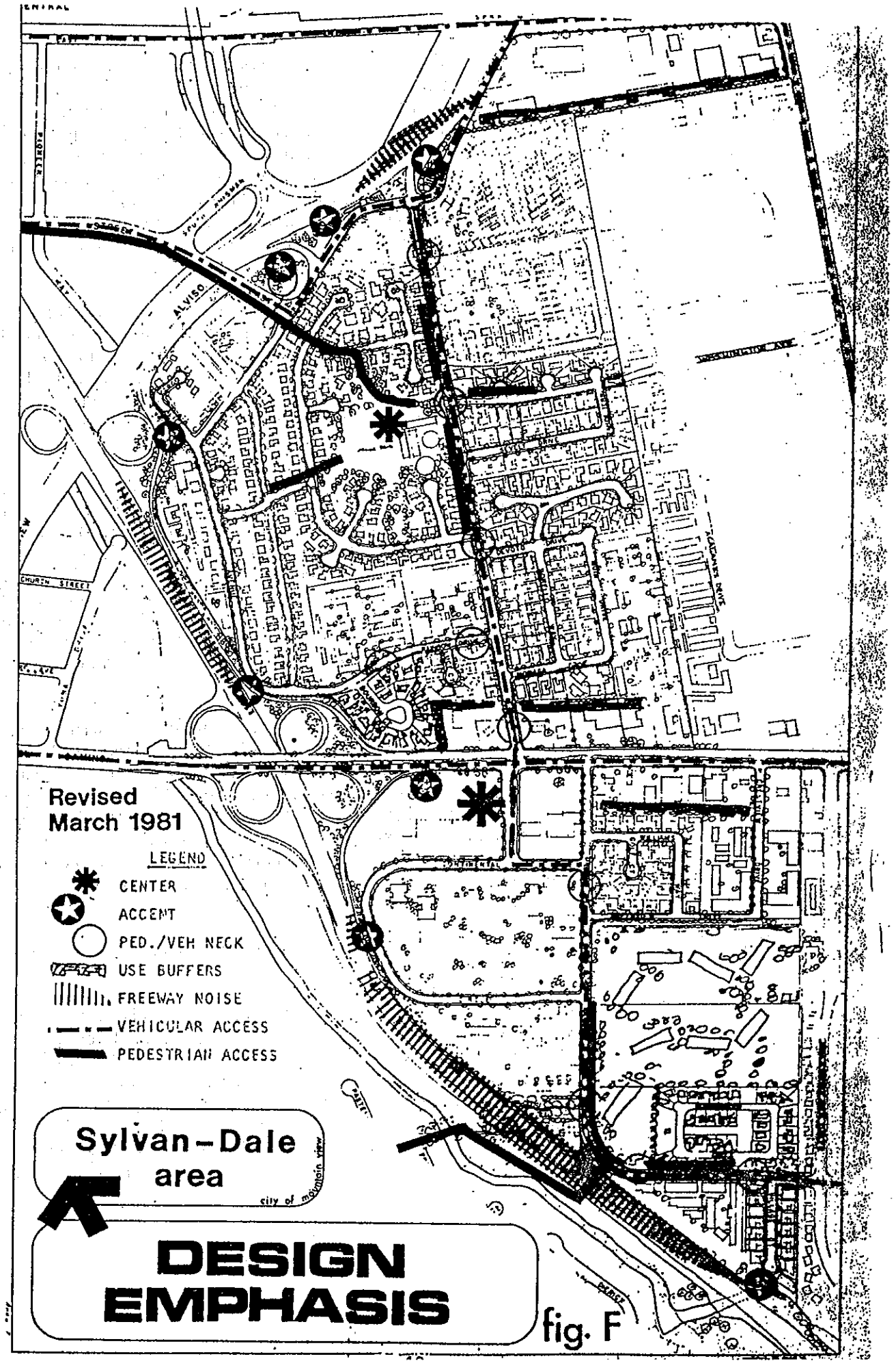
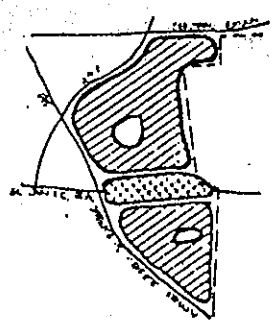

Revised
March 1981
LEGEND
CENTER
ACCENT
PED./VEH NECK
USE BUFFERS
FREEWAY NOISE
VEHICULAR ACCESS
PEDESTRIAN ACCESS
Sylvan-Dale
area
city of mountain view
ALVISO
CHURCH STREET
PIONEER
WASHINGTON AVE
DESIGN
EMPHASIS

fig. F

GENERAL FORM



The Sylvan-Dale Area is a distinctly defined portion of Mountain View which is divided into two primary residential areas, one north and one south of El Camino Real. These two areas are divided along El Camino by the high-intensity commercial activity. Within the Plan, the residential users surround open green areas which serve as focal points. A minor arterial carrying relatively heavy traffic separates each of the residential areas in a north-south direction, constituting both a physical and visual barrier within each of the neighborhood sections.

BARRIERS

The residential areas and commercial areas blend with similar uses in the City of Sunnyvale, although operationally there is little unity between the uses. Definitive barriers consisting of Central Expressway, Southern Pacific Railroad, Mountain View-Alviso Freeway and Stevens Creek Freeway bind the area on the other three sides. Together they produce a physical separation from the remainder of the community.

LANDSCAPE

The topography of the area is flat, with the exception of grading done for construction of freeways. Several surviving remnants of orchards remain, primarily on the school-owned properties, creating the existing feeling of openness. In addition to the orchards, several groupings of large trees, particularly pines, oaks and redwoods, are scattered throughout the Sylvan-Dale Area. Both the Santa Cruz and Diablo mountain ranges are visible from the area, with particularly important vistas along Sylvan Avenue, Dale Avenue, and at elevated points such as freeway overcrossings.

SCALE

The scale of development under the Plan would vary throughout the area, from one-story single-family and duplex structures to multi-level and high-rise buildings. Development is most intense near the freeways and along El Camino, generally dropping in intensity toward the open areas in the center of the two residential segments. The Emporium-Americana complex serves as the center of the most intense activity with high-density residential uses, high-rise buildings, and a commercial complex.

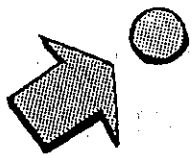
CONSTRAINTS

The Plan seeks to overcome several basic environmental constraints present within the area. The first of these constraints is noise and air pollution imposed on the environment, particularly by Highways 237 and 85. Tire, braking and starting noises on other arterials also contribute to these problems in direct proportion to the volume of traffic carried. The policies which follow are aimed at both removing the negative impact where possible and, secondly, mitigating the impact where existing conditions prevail. The second design constraint is the isolation of the Sylvan-Dale Area from the remainder of the community, again largely resulting from the barrier created

by the freeways. The separation, coupled with a lack of orientation and form for the existing development, should be overcome in order to provide the area with an individual character as well as an identitity linking it with the remainder of the City of Mountain View.

DESIGN POLICIES

DESIGN FOR ORIENTATION



The objects of design for orientation are to provide the sense of logical urban form and direction. The Land Use and Circulation sections provide the base for the compatible blending of types and intensities of uses. Emphasis is placed upon the design of public areas and facilities to implement the idea, tying together the private sector elements. The concepts of focal points and linkages are stressed to provide visual areas of interest and to guide the pedestrians.

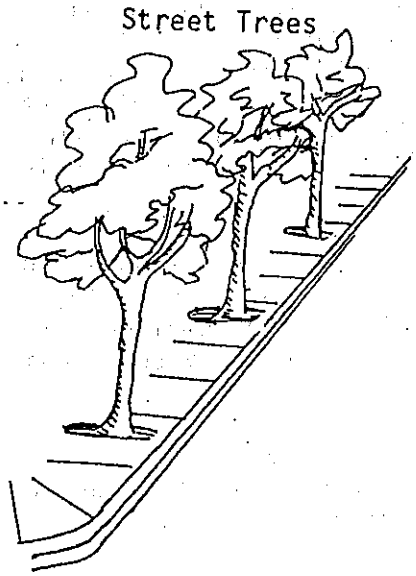
Centers of Activity

The two park or open space areas serve as the primary centers in each of the residential neighborhoods. Design of uses adjacent to these centers is such that the surrounding uses do not infringe or block the visual or physical access into the parks. The intensity of use gradually increases as it radiates from the open areas, reaching a maximum in the Americana-Emporium complex. The proposed high-rise buildings will also provide a central point for physically locating oneself within the Sylvan-Dale Area.

Paths

Paths for pedestrian and bicycle use will radiate from the parks down into the rest of the residential area. Special emphasis in the use of surface materials and the landscaping of these paths will serve as an introduction prior to reaching the parks.

Street Trees



Implementation of a complete street tree system can be one of the most effective means of enhancing the neighborhood and providing a sense of orientation and identity. Trees of the same variety, spaced along either side of a street, provide a sense of continuity and direction. Use of large varieties of trees, especially along wide streets, reduces the scale and softens the impact of the street, making it more comfortable for pedestrian and bicycle use. In order to fully accomplish the linkage, species of trees cannot be indiscriminately mixed or the sense of continuity and direction is lost.

Street trees need not be rigidly placed at intervals but should be fitted to the adjacent uses. An average spacing of 40 feet with a maximum distance of 60 feet is recommended for most streets. Closer spacing or clustering of the street trees is desired on major streets and is planned for Sylvan, Dale, Evelyn, and El Camino Real. The El Camino design is in accord with the overall design to be followed for the entire street in Mountain View.

Within new developments, street trees will be added automatically. On major streets such as Sylvan, special street tree designs will be included as part of the project (Exhibit J). The most difficult problem may be completion and enhancement of existing residential streets. It is recommended that a City-wide reinvestment program to complete streets be provided for during the next five years. Effectuation of this reinvestment program can be designed to follow each succeeding area plan. The program should contain, as a minimum, the following elements:

a. Sufficient funds for materials and manpower.

b. A definitive set of guidelines for "filling in" existing street tree mixtures.

c. A well-defined public information and notification procedure. Appendix C lists each of the streets in the Sylvan-Dale Area and evaluates the degree of completion of the street tree program on each street as of 1973.

Accent Landscaping

Groupings of large trees are recommended to identify points of activity or interest. Dense clusters of street trees signal the entrances to the City on El Camino, Heatherstone, Dana and Evelyn. Large species of trees planted in a park can serve as an open space marker to surrounding uses. Finally, planting of large clusters of trees can serve to identify the edges of the Sylvan-Dale Area, particularly where access is limited. Clusters of tall conifers are recommended for use adjacent to the freeways, both to shield and to soften this harsh edge of the Sylvan-Dale Area. Dramatic coloring, foliage, or flowers are to be utilized within the park areas near entrance points to denote the points of change in intensity of use. Finally, in the Sylvan-Dale Area, the tree growth preserved as part of the Emporium complex not only provides a pleasant open space, but identifies the commercial use and entrance to both the City and the Sylvan-Dale Area.

Private on-site landscaping should pick up themes established along the street with respect to materials. An an example, use of trees designated for street trees in the remainder of a landscape design can enhance the continuity and visual design of the neighborhood.

DESIGN FOR NOISE AND AIR POLLUTION

As will be described more thoroughly in the section under street designs, particular attention is given to discouraging vehicular movement where it conflicts with residential uses, and to encouraging its flow where it will have the least impact.

Traffic Orientation

It has been the policy of the transportation section of this Plan to direct the traffic away from residential uses. Those lands which lie directly adjacent to the freeway are, for the most part, designated for open uses, unless already substantially developed. The sd zoning district has been applied to lands where noise remains a potential problem and special design will be required in the form of setbacks of other mitigating factors.

Freeway Buffers

Along the freeways, increased dense landscaping and sound walls within the State right-of-way are recommended at State cost. While new freeways are being built with noise reduction considerations, no mechanism is currently available for remedial work; however, means may be available within the 10-year period of this Plan. In the interim, buffers should be established on adjacent lands, both public and private, as part of any new construction. State lands in the Sylvan Area north of Moorpark will also serve as landscape buffers.

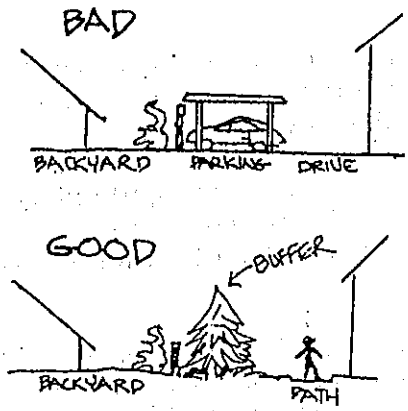
Traffic Buffers

Uses adjacent to portions of Moorpark and Sylvan may also need sound walls. The policy of this Plan is to allow special consideration of setback and yard requirements where noise and traffic are a problem. For new construction, shielding will be required as part of a subdivision or architectural approval. For existing uses, a variance may be reasonably considered for a wall up to 6 feet in height, extending to within 5 feet of the right-of-way so long as sight distance problems are not created.

In either case, the walls must be visually attractive, compatibly designed with each other and combined with effective landscaping. Failure to observe these three criteria will result in an unattractive, wall-lined street. If properly designed, the walls will provide a visual linkage within the area. The acoustic effect of the wall design on adjoining properties must be minimized through design. Curvilinear walls, for instance, do not reflect noise as do straight walls. Heavy landscaping may muffle sounds and some materials may more readily absorb sound.

Noise from Land Use

Street design can improve the impact of sounds on adjacent uses by placing traffic away from the edges of the right-of-way. Bike lanes, planter strips and sidewalks which utilize the portion of the right-of-way near the property increase the distance between the traffic and the property. Heavy landscaping or street trees on the sides of streets minimizes the physical and psychological impact of noise. Such ideas are incorporated into proposed plans for Sylvan and Moorpark.



Many of the above solutions, particularly those relating to walls and distance buffers, can reduce noise between different intensities of use. Of particular concern is the relationship of commercial and industrial uses to residential uses. Implementation of a light industrial zone along Evelyn Avenue will aid in achieving types of industrial uses which are compatible with adjacent residential uses. In addition, site design can insure that noise-generating activities such as delivery, parking, trash pickup, and equipment are located away from adjacent light-intensity uses. As an example, parking for an apartment complex should not be directly adjacent to the rear yards of low-density residences. Instead, pedestrian walkways or open areas would provide a more compatible relationship of uses, visually as well as with respect to noise. Remaining areas where buffering is needed are noted on Figure F and include the industrial uses along Evelyn and the commercial uses along El Camino.

DESIGNS FOR OPEN SPACE

Open space can be measured both by acreage and by "feel." The Cherry Chase Golf Course and public parklands provide the major quantities of open space land devoted to open urban uses. The design of streets and building sites provides the overall feeling of openness in the Sylvan-Dale Area. Eventual loss of the orchards will remove both the acreage and feeling now present in the area.

Park Design

Since the park and golf course area remain as the visual centers of the two residential areas, design of adjacent uses should be at a scale to enhance the parks, not shield them. Landscaped access paths shall extend out from the park.

The park shall incorporate large trees to signal the park's existence from a distance. Emphasis on large tree clusters can serve as a means of reducing maintenance costs that could be incurred from a more formal open park area. These wooded areas will serve to retain the open feeling created by the orchards. Orchard trees should be retained on park site during transition to final development.

Sylvan Park

Sylvan Park, at approximately 09 acres, will include active and passive recreational areas, including family picnic, playground, and children's play areas. In light of recent consideration of neighborhood park costs, an estimated $250,000 to $300,000 will be required for full development.

Cost

Ideally, the land will incorporate the park and school in a unified school-park relationship. The surrounding residential areas will flow into the park. The opportunity exists under a combined public facilities and planned unit development approach to integrate public and private uses. As an example, a homeowners association may wish to construct a swim club or cabana in conjunction with the park. In the end, school, private and City areas would be fully merged.

Traffic Orientation

It has been the policy of the transportation section of this Plan to direct the traffic away from residential uses. Those lands which lie directly adjacent to the freeway are, for the most part, designated for open uses, unless already substantially developed. The sd zoning district has been applied to lands where noise remains a potential problem and special design will be required in the form of setbacks of other mitigating factors.

Freeway Buffers

Along the freeways, increased dense landscaping and sound walls within the State right-of-way are recommended at State cost. While new freeways are being built with noise reduction considerations, no mechanism is currently available for remedial work; however, means may be available within the 10-year period of this Plan. In the interim, buffers should be established on adjacent lands, both public and private, as part of any new construction. State lands in the Sylvan Area north of Moorpark will also serve as landscape buffers.

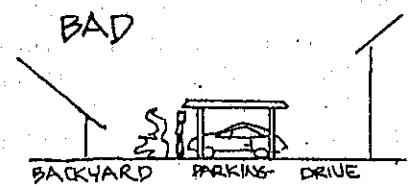
Traffic Buffers

Uses adjacent to portions of Moorpark and Sylvan may also need sound walls. The policy of this Plan is to allow special consideration of setback and yard requirements where noise and traffic are a problem. For new construction, shielding will be required as part of a subdivision or architectural approval. For existing uses, a variance may be reasonably considered for a wall up to 6 feet in height, extending to within 5 feet of the right-of-way so long as sight distance problems are not created.

In either case, the walls must be visually attractive, compatibly designed with each other and combined with effective landscaping. Failure to observe these three criteria will result in an unattractive, wall-lined street. If properly designed, the walls will provide a visual linkage within the area. The acoustic effect of the wall design on adjoining properties must be minimized through design. Curvilinear walls, for instance, do not reflect noise as do straight walls. Heavy landscaping may muffle sounds and some materials may more readily absorb sound.

Noise from Land Use

Street design can improve the impact of sounds on adjacent uses by placing traffic away from the edges of the right-of-way. Bike lanes, planter strips and sidewalks which utilize the portion of the right-of-way near the property increase the distance between the traffic and the property. Heavy landscaping or street trees on the sides of streets minimizes the physical and psychological impact of noise. Such ideas are incorporated into proposed plans for Sylvan and Moorpark.



Many of the above solutions, particularly those relating to walls and distance buffers, can reduce noise between different intensities of use. Of particular concern is the relationship of commercial and industrial uses to residential uses. Implementation of a light industrial zone along Evelyn Avenue will aid in achieving types of industrial uses which are compatible with adjacent residential uses. In addition, site design can insure that noise-generating activities such as delivery, parking, trash pickup, and equipment are located away from adjacent light-intensity uses. As an example, parking for an apartment complex should not be directly adjacent to the rear yards of low-density residences. Instead, pedestrian walkways or open areas would provide a more compatible relationship of uses, visually as well as with respect to noise. Remaining areas where buffering is needed are noted on Figure F and include the industrial uses along Evelyn and the commercial uses along El Camino.

DESIGNS FOR OPEN SPACE

Open space can be measured both by acreage and by "feel." The Cherry Chase Golf Course and public parklands provide the major quantities of open space land devoted to open urban uses. The design of streets and building sites provides the overall feeling of openness in the Sylvan-Dale Area. Eventual loss of the orchards will remove both the acreage and feeling now present in the area.

Park Design

Since the park and golf course area remain as the visual centers of the two residential areas, design of adjacent uses should be at a scale to enhance the parks, not shield them. Landscaped access paths shall extend out from the park.

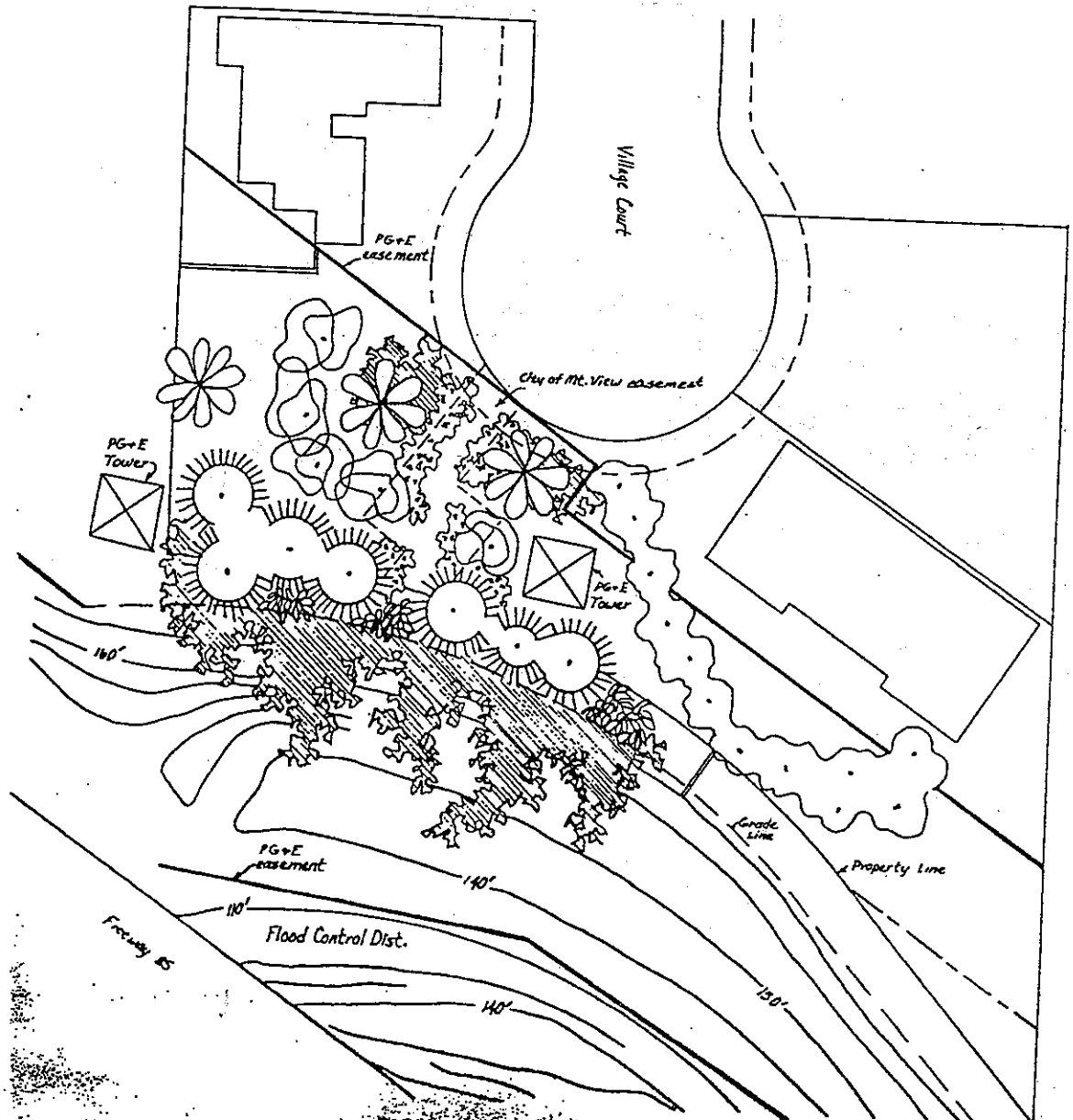
The park shall incorporate large trees to signal the park's existence from a distance. Emphasis on large tree clusters can serve as a means of reducing maintenance costs that could be incurred from a more formal open park area. These wooded areas will serve to retain the open feeling created by the orchards. Orchard trees should be retained on park site during transition to final development.

Sylvan Park

Sylvan Park, at approximately 12 acres, will include active and passive recreational areas, ~~including~~ family picnic, playground, and children's play areas. In light of recent consideration of neighborhood park costs, an estimated $250,000 to $300,000 will be required for full development.

Cost

Ideally, the land will incorporate the park and school in a unified school-park relationship. The surrounding residential areas will flow into the park. The opportunity exists under a combined public facilities and planned unit development approach to integrate public and private uses. As an example, a homeowners association may wish to construct a swim club or cabana in conjunction with the park. In the end, school, private and City areas would be fully merged.



Quercus douglasi (California Blue Oak)
Pinus nigra (Austrian Pine)
Magnolia grandiflora (Southern Magnolia)
Choisya ternata (Mexican Orange)
Trachelospermum jasminoides (star jasmine)
Hedera (Ivy)

Sylvan-Dale Urban Design Sketch
Prepared by the City of Mountain View Planning Dept.

figure H

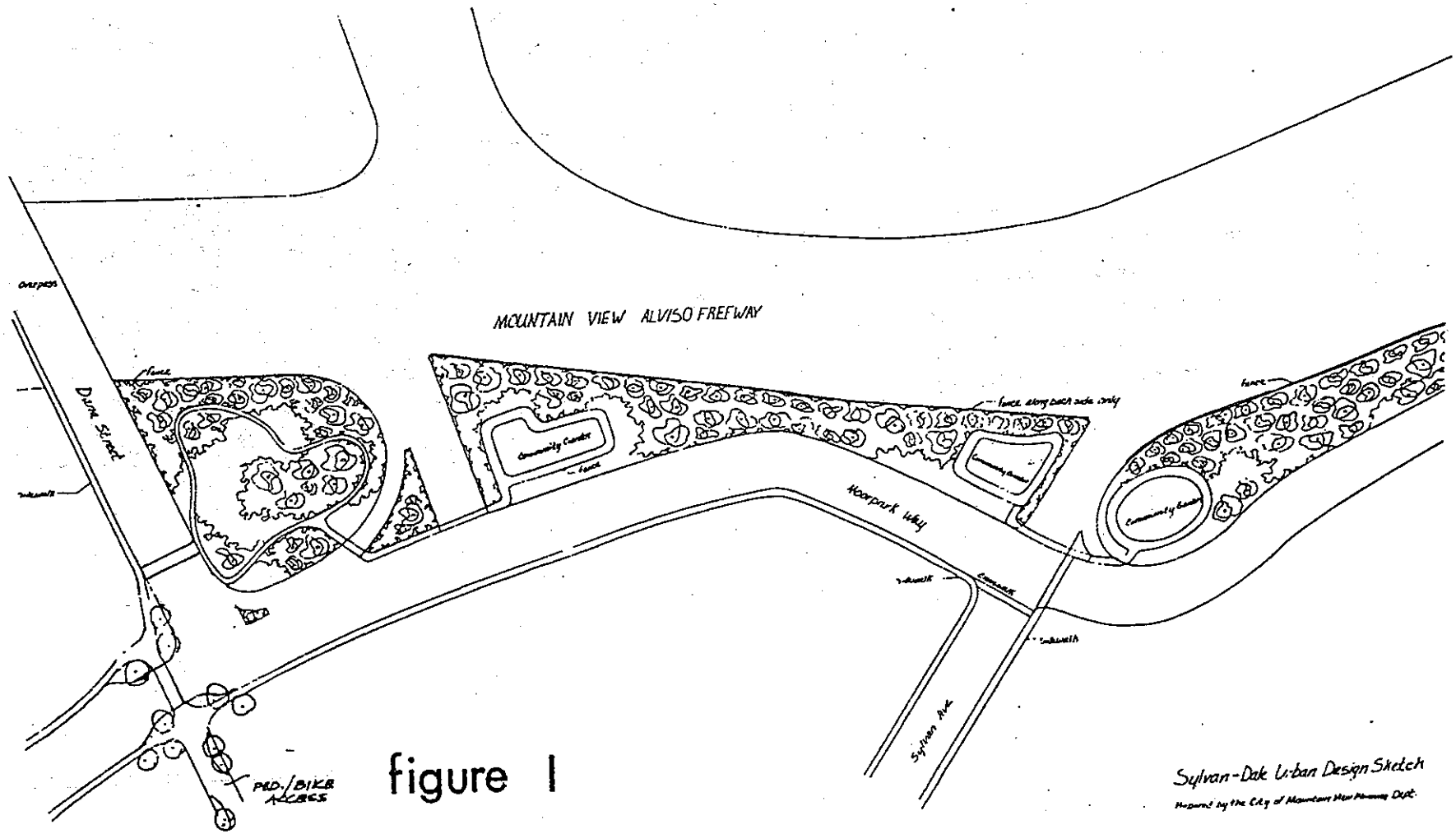


figure 1

Sylvan-Dale Urban Design Sketch

Prepared by the City of Mountain View Planning Dept.

Neighborhood Approval	Preliminary architectural drawings should be prepared and concepts presented to residents of the area prior to the time that final design is accepted. The citizens' participation is essential to insure that the park serves the needs of the residents.
Special Accents	Several areas within the Sylvan-Dale Area lend themselves to special landscaping treatments. Presently barren or unkempt, dusty areas, the areas designated in Figure F as landscape accents serve as buffers, new visual open space, and potential community involvement project areas. A particular area of interest is the land at the terminus of Village Court. These privately-held lands are covered by a PG&E wire clearance easement so that no structures may be built within the area. The City maintains a 20 foot wide easement from the terminus of Village Court to the flood control right-of-way adjacent to Stevens Creek. Figure G depicts a possible landscape design for this area assuming the cooperation of the property owners. In addition to landscape and buffering benefits, the area could be utilized as a tot lot by residents of the block. The Plan envisions future access to Stevens Creek Park Chain along the easement, but details of such access are beyond the scope of this Plan.
Landscaping	Private on-site landscaping of intensities of use is one of the best means of creating the sense of natural open space.
SPAR Review	Site Plan and Architectural Review procedures required by the City for all multiple-family, commercial and industrial uses help to ensure the use of landscaping to soften the harsh aspects of parking and building design. The lower-density uses, on which this Plan places major emphasis, do not presently require such public attention to on-site landscaping. Unconscious, unrelated landscaping, use of excessive amounts of concrete, asphalt and block, and a disregard for the shielding or merging capabilities of the landscaping are all evident in existing low-density developments. The Plan recommends that a limited SPAR (Site Plan and Architectural Review) procedure be extended to R1 and R2 subdivisions.
Interim Uses	During the period of this Plan, much of the public and private lands will remain in nonurban uses. Where existing orchards are still viable, the Plan encourages that they be maintained and operated. Private flower growing is also encouraged on privately held or publicly-held vacant lands to introduce an attractive, open use of the lands until such time as they are developed. A final alternative for vacant land is community vegetable or flower gardens. This specific use is recommended for the Highway Department lands adjacent to Highway 237 and should be made available principally to the residents of the apartment complexes or mobile home developments (Figure H).

MAJOR STREET DESIGN

The Plan calls for special design of portions of Sylvan, Dale and Moorpark. The designs are aimed at facilitating traffic flow with minimum impact upon surrounding land uses.

Sylvan Avenue

Medians

Figures J-a and J-b depict proposed conceptual geometrics for Sylvan. The nominal 70 foot right-of-way is retained throughout and few modifications are made to already existing improved sections of the street. A landscape median is added at the intersection of Sylvan with El Camino Real, completing the four streets of that intersection with medians and picking up the landscape theme. A median is also added at the intersection with Moorpark. Both medians served to provide traffic control with respect to left-turn lanes. In addition, they serve as visual signals to the residential areas and break up otherwise unrelieved stretches of street. Parking is eliminated in much of the proposed new section since uses do not directly abut the street. For the most part, parking is provided adjacent to existing residential uses. The added width obtained by elimination of parking is utilized to physically minimize the width of the street and to minimize the noise impact on adjacent sidewalks, and serves both to extend the park in either direction and to provide improved pedestrian facilities. The width of lanes, landscape design, and "feeling" of the street are intended to encourage slower speeds through this residential area while maintaining an acceptable level of service for through traffic.

Pedestrian Facilities

Special pedestrian facilities are provided along and across Sylvan Avenue. As the street approaches the park, pedestrian peninsulas are provided to minimize the walking distance across the street. In addition, they serve to signal the entrance from a high-volume street into a residential area. These same landscape peninsulas are also provided at special pedestrian crossings in the vicinity of the proposed park and serve to enhance access for the handicapped.

Moorpark Way

Moorpark Way is designed to encourage a flow of traffic north from the terminus of Dana Street and to discourage movement to the south past residential areas. Pedestrian crossing provisions on the southerly side act as constriction to traffic flow and signal the residential area.

Pedestrian Peninsulas

New residential streets abutting major streets such as Sylvan will utilize pedestrian peninsulas to denote an entrance to a residential area (see Figure J, Exhibit A). Peninsulas will also serve to keep intersections clear and reduce the distance pedestrians must traverse a street. The Plan calls for reconstruction of existing corners at Rainbow, Moraga, Devoto and Sevely when the remainder of the street is completed.

New streets shall discourage straight, "shotgun" appearance. Landscape projections into parking areas are required to shield on-street parking. Projections will also be needed at T-intersections where shielding of abutting residences from headlights

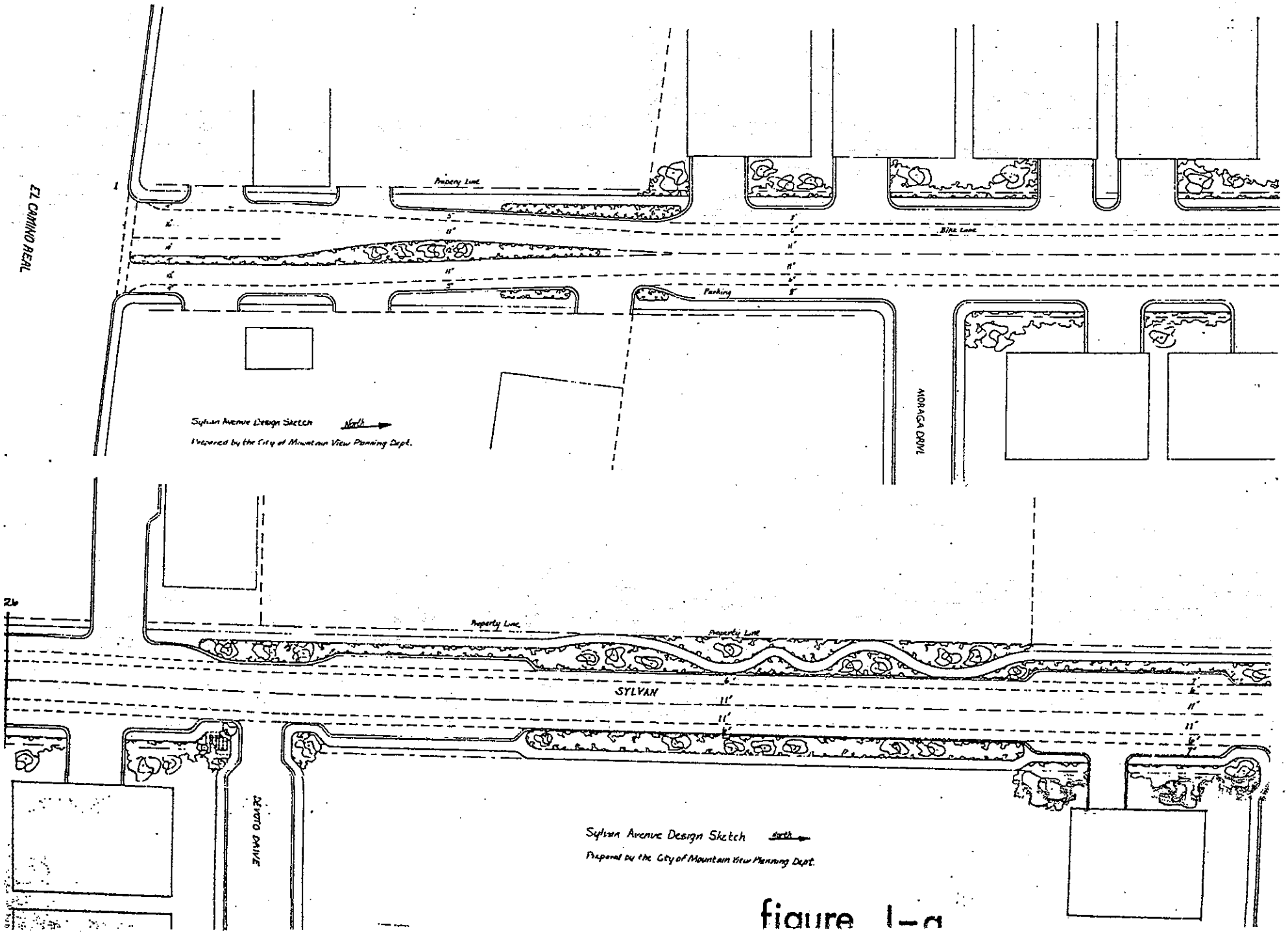
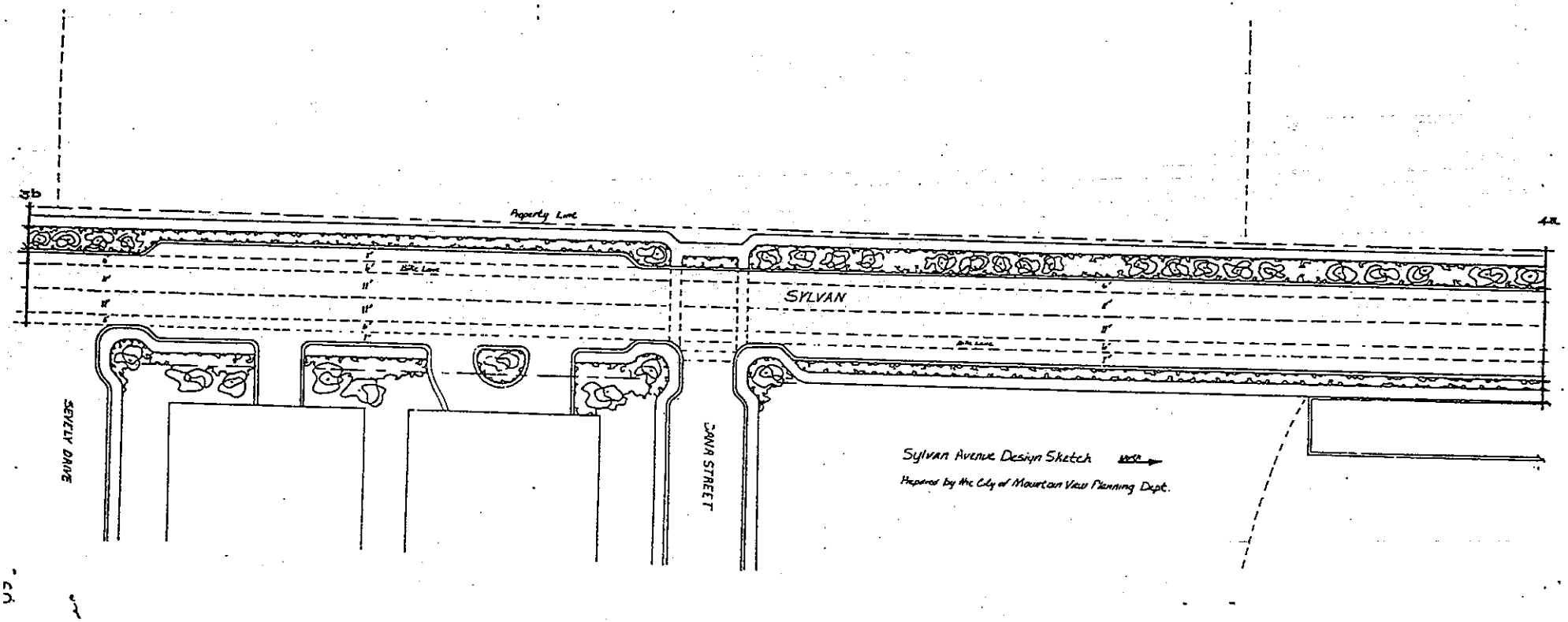

EL CAMINO REAL
Property Line
Bike Lane
Parking
MORAGA DRIVE
Sylvan Avenue Design Sketch
North
Prepared by the City of Mountain View Planning Dept.
Property Line
Property Line
SYLVAN
DE SOTO DRIVE
Sylvan Avenue Design Sketch
North
Prepared by the City of Mountain View Planning Dept.

figure 1-a


Property Line
SYLVAN
SEVELY DRIVE
LANA STREET
Sylvan Avenue Design Sketch
Prepared by the City of Mountain View Planning Dept.

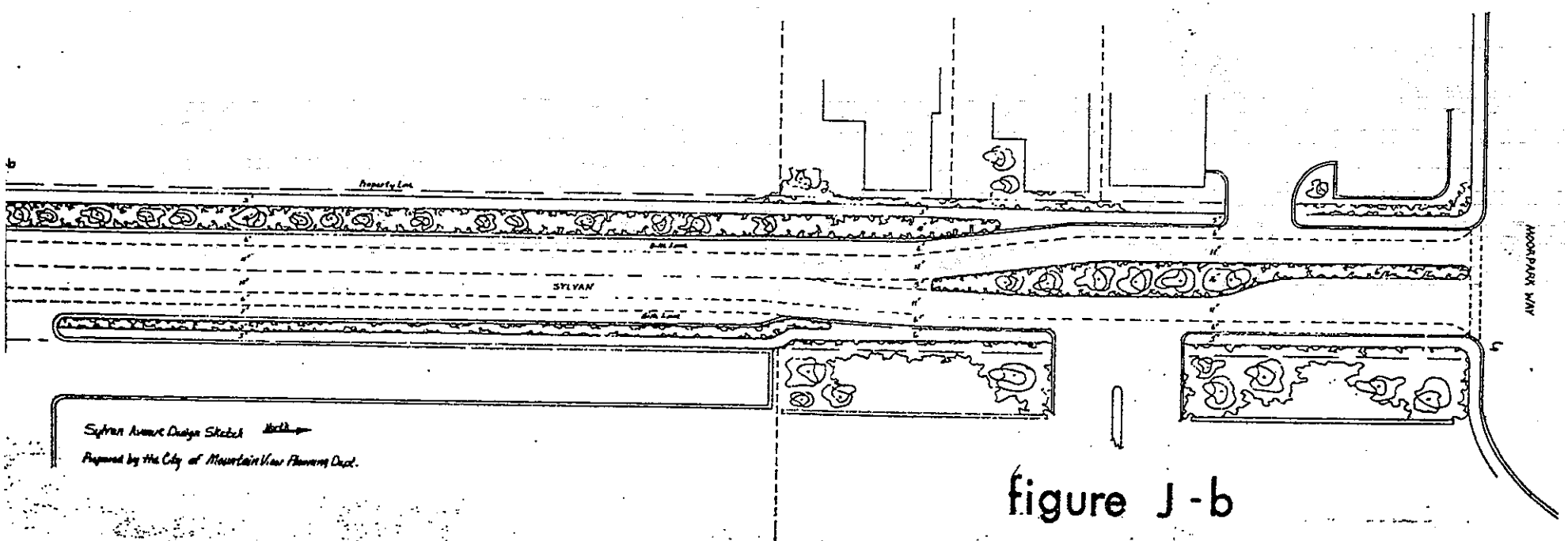
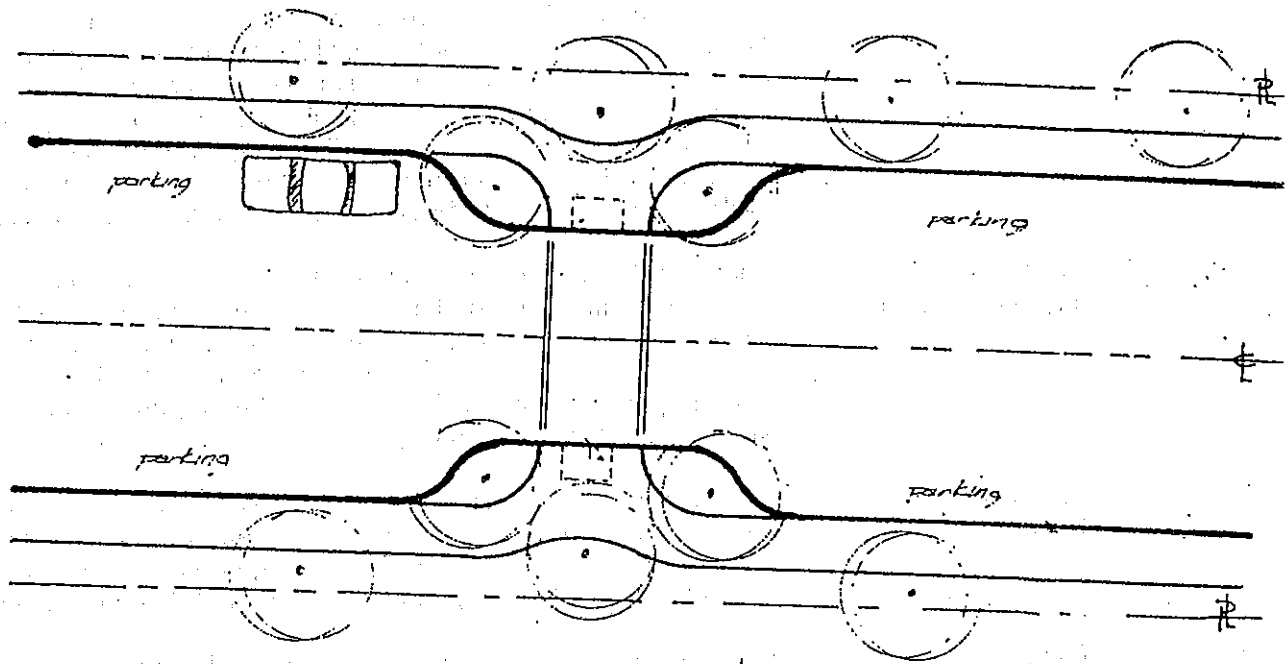

Property Line
SYLVAN
HOOKPARK WAY
Sylvan Avenue Design Sketch
Prepared by the City of Mountain View Planning Dept.

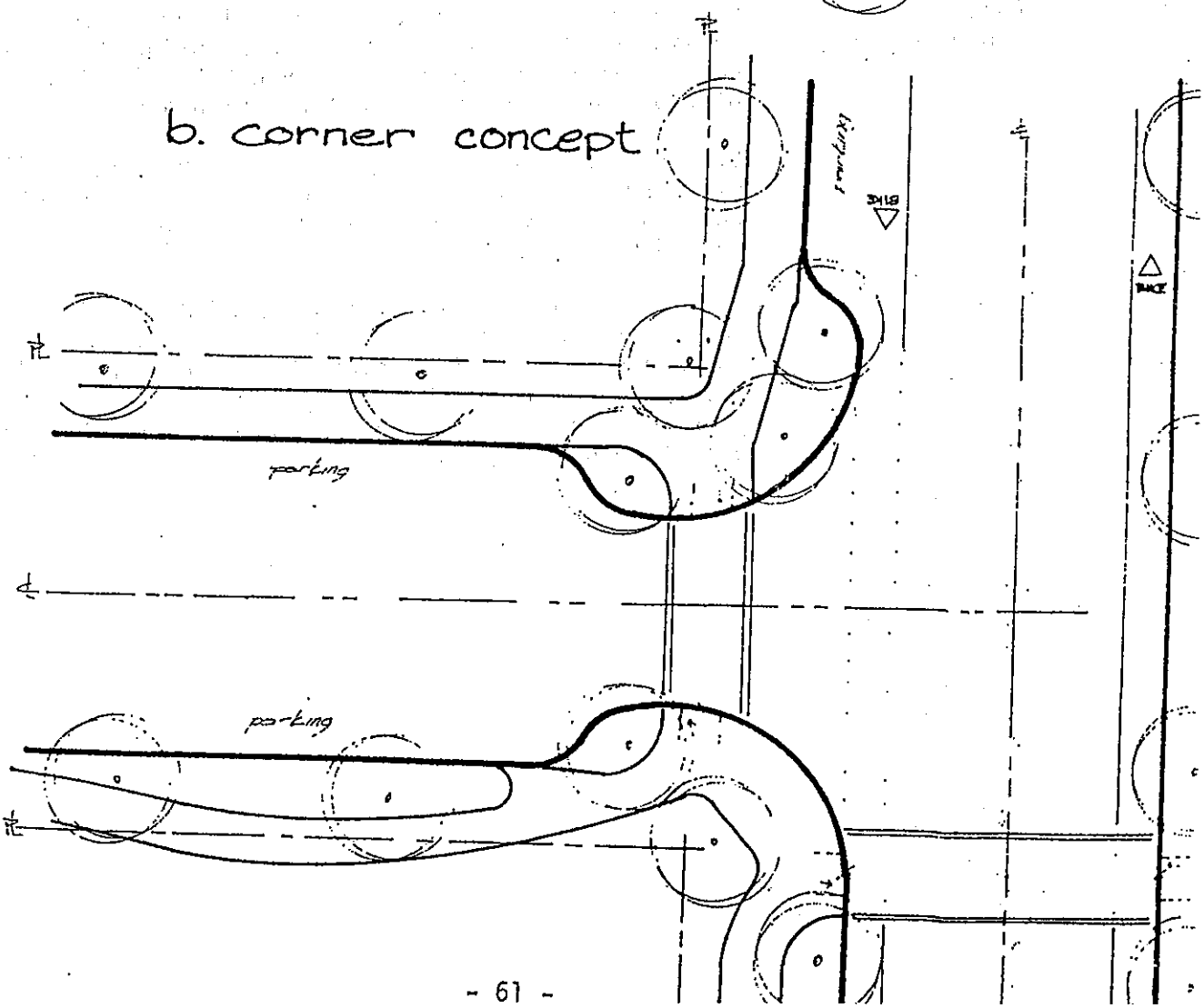
figure J-b

Pedestrian / Vehicle "Neck"

a. - mid block concept



b. corner concept



is desirable. These improvements shall be accomplished as part of subdivision approval.

These same improvements can and should be made to existing streets; however, the costs for such changes should be assessed on the subdivisions themselves. The simplest mechanism for this to proceed is for local residents to ask for a civic improvement and assessment district to be formed. Cost of the improvements would then be evenly dispersed to all property owners if a majority agree. The landscaping of such projects could be part of a neighborhood volunteer effort which could further reduce costs and stimulate citizen participation.

Design Implementation

Implementation of the recommended design concepts will require the coordination of the public and private development activities. It will also require continued awareness and participation of the owners and residents of the area. Specific techniques and priorities have been suggested throughout the section. The following is a summary of the priorities.

Precise Street Plans

A precise geometric design is needed for Sylvan Avenue to guide development over the next five years. The street should be fully completed, including changes to the existing areas, by the time the first phase of the park is completed.

Capital Improvements

Capital improvement scheduling will have a major effect in the phasing of development. Of highest priority are (1) development of the first phase of Sylvan Park, (2) Sylvan Avenue completion, and (3) provision for design accents.

Site review is an ongoing process. However, additions to the City ordinances which will provide a means to review major residential developments, in accord with new residential zones, should be undertaken.

One of the most important aspects of design implementation is the assurance that talented design professionals are employed to complete specific developments in the area. Using the guidance of this Plan, architects can design both public and private developments with greater attention to the total effect upon the Sylvan-Dale Area.

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APPENDICES

Appendix A

The original Plan contained a suggested list of rezoning actions, most of which have been implemented. The list has been deleted from this printing and replaced with a current zoning map.

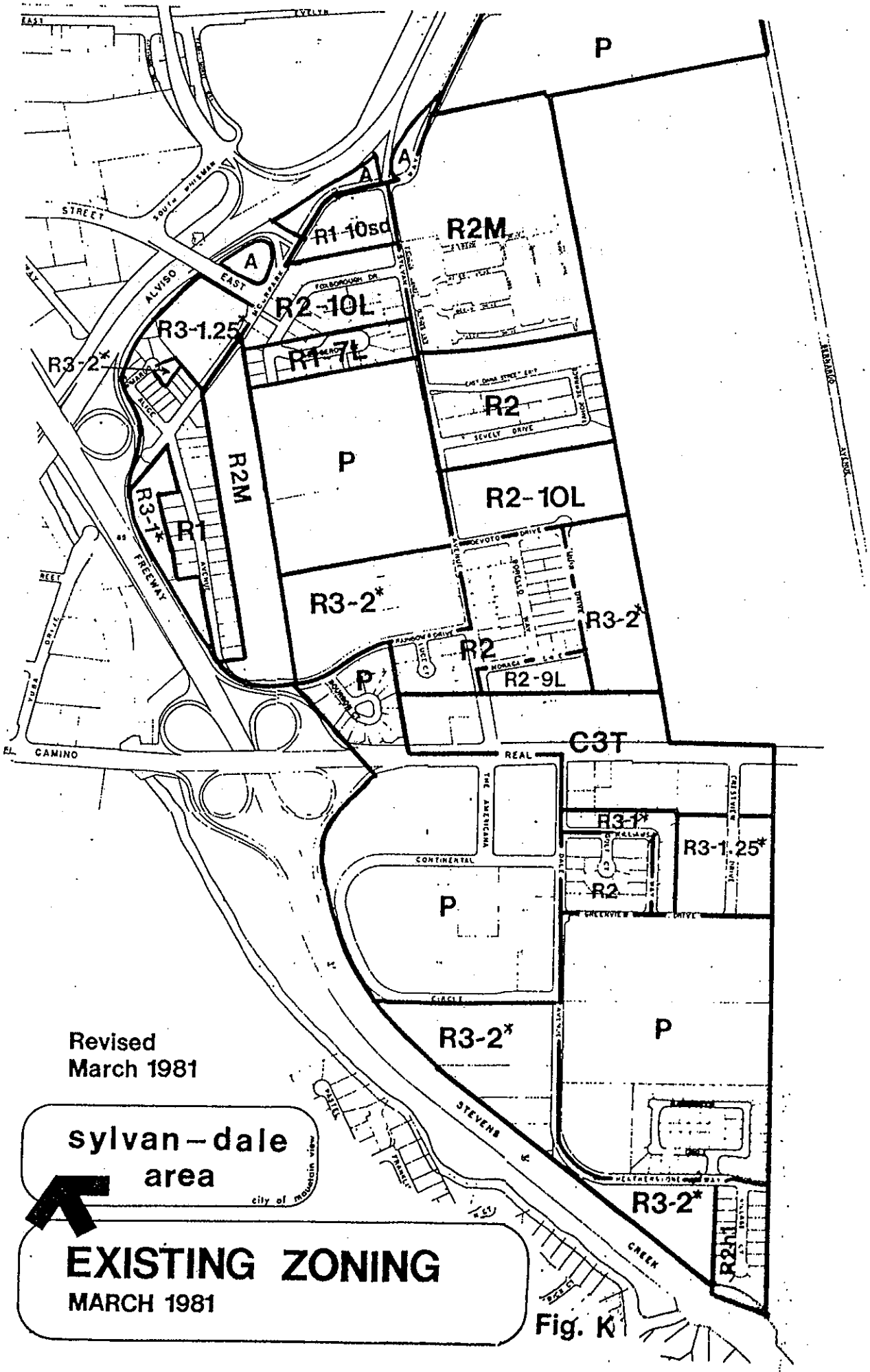
P
R2M
R1-10sd
A
R2-10L
R3-1.25
R1-7L
R3-2*
R2
P
R2M
R3-1*
R1
R2-10L
R3-2*
R3-2*
R2
R2-9L
P
C3T
EL CAMINO REAL
R3-1*
R3-1.25*
R2
P
R3-2*
P
R3-2*
R2-1
ALVISO STREET
EAST
FREEWAY
STEVENS CREEK
Revised
March 1981
sylvan–dale
area
city of mountain view
EXISTING ZONING
MARCH 1981

Fig. K

Appendix B

LIST OF STREET TREES

Street	Species	Degree of Completion
Alice Avenue	Moraine Locust (Gleditsia Triacanthos Inermis) Deciduous	Poor
Bernardo Avenue	Southern Magnolia (Magnolia Grandi-flora) Evergreen	Poor
Borello Way	Sweet Gum (Liquidambar Styraciflu) Deciduous	Poor-Mixed
Continental Circle	Sweet Gum (Liquidambar Styraciflu) Deciduous	Complete
Dale Avenue	Southern Magnolia (Magnolia Grandi-flora) Evergreen	Partial
East Dana Street (Washington)	Southern Magnolia (Magnolia Grandi-flora) Evergreen	Partial
Devoto Street	Caroline Cherry Laurel (Prunus Caroliniana) Deciduous	Partial
El Camino Real East	Caroline Cherry Laurel (Prunus Caroliniana) Deciduous	Complete
Evelyn Avenue	Moraine Locust (Gleditsia Triacanthos Inermis) Deciduous	Partial
Golf Court	Southern Magnolia (Magnolia Grandi-flora) Evergreen	Good
Greenview Drive	Caroline Cherry Laurel (Prunus Caroliniana) Deciduous	Good
Heatherstone Way	Southern Magnolia (Magnolia Grandi-flora) Evergreen	Partial

Street	Species	Degree of Completion
Luce Court	Sweet Gum (Liquidambar Styraciflu) Deciduous	Poor
Margo Drive	Southern Magnolia (Magnolia Grandi-flora) Evergreen	Poor
Moorpark Way	Southern Magnolia (Magnolia Grandi-flora) Evergreen	Poor
Moraga Drive	Sweet Gum (Liquidambar Styraciflu) Deciduous	Poor
Muir Drive	Caroline Cherry Laurel (Prunus Caroliniana) Deciduous	Poor
Rainbow Drive	Sweet Gum (Liquidambar Styraciflu) Deciduous	Partial
Sevely Drive	Goldenrain (Koelreuteria Paniculata) Deciduous	Partial
Sylvan Avenue	Chinese Pistache (Pistacia Chinensis) Deciduous	Complete
The Americana	Chinese Pistache (Pistacia Chinensis) Deciduous	Complete
Tahoe Terrace	Goldenrain (Koelreuteria Paniculata) Deciduous	Partial
Village Court	Southern Magnolia (Magnolia Grandi-flora) Evergreen	Poor
Williams Way	Caroline Cherry Laurel (Prunus Caroliniana) Deciduous	Good

Appendix C

POTPURRI

For various reasons, many ideas and concepts related to neighborhood do not fit neatly into the above topics. The following list is a grab bag of ideas, standards, and criteria to further implement the Plan:

1. Communication

Quite often residents of an area do not know about impending developments within their neighborhood. Rezonings, special zoning actions and public improvements typically are noticed to adjacent properties, often limited to 200 feet. Notifying people much beyond that radius becomes an overwhelming task considering the resources available.

The Plan recommends a standing notice or mailing list to be established consisting of citizens who want to be informed about those specific actions happening within the Sylvan-Dale Area. This list would be used in addition to the standard mailing list for any important actions within the neighborhood.

A second alternative is to use registered voter lists for major developments.

2. Special Private Open Space

The Plan encourages the cooperative efforts of citizens living within the area to form additional quasi-public facilities such as swim clubs or cabanas or tennis clubs. Residents of a duplex subdivision could enter into common agreements to purchase and develop lands for such purposes for the exclusive use of their membership. Such facilities would need to obtain a Conditional Use Permit.

3. Use of Private Recreation Facilities

The Sylvan-Dale Area has as part of its mobile home park and apartment developments at least 12 swimming pools and 15 recreation or meeting rooms. Several additional meeting facilities are to be added as part of industrial and commercial uses. The Plan recommends use of these facilities for Parks and Recreation Department programs to service the Sylvan-Dale Area, particularly until such time as public park lands can be provided.

4. Citizen Implementation Programs

Reference has been made several times to the possibility of citizen involvement/volunteer programs for accomplishment of neighborhood projects. Such things as coordinated street tree landscaping or landscaping of vacant City lands could be accomplished by this means. Unless funding for these involvement programs is provided, the interest and momentum of a group of citizens

can wane or be lost. The Plan recommends that provisions be included in the yearly budget for such use. The existence of such a fund may well stimulate citizen interest.

5. Public Lands Contract

The Plan suggests temporary or interim use of public lands specifically for community gardens. In order to facilitate interest in this idea, a standard contract and procedure will need to be established by the City.

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Appendix D

COMPILATION OF AMENDING RESOLUTIONS

RESOLUTION NO. 10081
Series 1974

A RESOLUTION ADOPTING A DETAILED POLICY ON LAND USE, CIRCULATION AND ENVIRONMENTAL DESIGN FOR THE SYLVAN-DALE AREA AND CERTIFYING THE ENVIRONMENTAL IMPACT REPORT

WHEREAS, a proposed Sylvan-Dale Area Plan was prepared by the Planning Staff of the City of Mountain View after consultation with residents and property owners of that area and other interested persons; and

WHEREAS, public hearings were held on said proposed plan before the Environmental Planning Commission of the City and the Environmental Planning Commission recommended adoption of said plan; and

WHEREAS, public hearings were held before the City Council on said proposed plan; and

WHEREAS, the City Council has received and considered an Environmental Impact Report on the impact of the adoption of the proposed plan; and

WHEREAS, the City Council has made such amendments to the proposed plan as it deems appropriate;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Mountain View as follows:

1. It is hereby certified that the Environmental Impact Report for said project as presented to the City Council has been completed in compliance with the California Environmental Quality Act of 1970, as amended, and with applicable state and local guidelines; that the City Council has reviewed and considered the information contained in the Environmental Impact Report; and the Environmental Impact Report is hereby adopted.

2. The Sylvan-Dale Area Plan as amended by the City Council is hereby adopted as a detailed policy on land use, circulation, environmental design and other factors for the Sylvan-Dale area.

3. The City Council hereby finds and determines that said Sylvan-Dale Area Plan is consistent with the General Plan of the City of Mountain View.

The foregoing Resolution was regularly introduced and adopted at an Adjourned Regular Meeting of the City Council of the City of Mountain View, duly held on the 20th day of May, 1974, by the following vote:

AYES: Councilmembers Allen, Anderson, Jelavich, Moss, Perez and Mayor Cusimano

NOES: None

ABSENT: Councilman Gordon
NOT VOTING: None

ATTEST:

JEAN HIXSON
CITY CLERK

APPROVED:

JOSEPH CUSIMANO
MAYOR

I do hereby certify that the foregoing resolution was passed and adopted by the City Council of the City of Mountain View at an adjourned regular meeting held on the 20th day of May, 1974 by the following vote.

City Clerk
City of Mountain View

RESOLUTION NO. 10288
Series 1974

A RESOLUTION MODIFYING THE SYLVAN-DALE AREA PLAN IN THE MATTER OF DENSITY AND CRITERIA FOR DEVELOPMENT OF THE LANDS ALONG HEATHERSTONE WAY

The City Council of the City of Mountain View does resolve as follows:

THAT, after receiving the recommendation of the Environmental Planning Commission and holding a public hearing thereon, the City Council does hereby amend the Sylvan-Dale Area Plan in the matter of density and criteria for development, under the Planned Community (P) zone, of the lands along Heatherstone Way bounded by the Mountain View-Sunnyvale City limits, the lands held for a neighborhood park and the golf course; said plan being amended as follows:

1. That the density of said parcel be limited to 44 units;
2. That the development of that parcel take into consideration the relationship with the golf course on one side and the City park on the other side;
3. That the architecture of the development take into consideration the relationship with the surrounding properties, both developed and undeveloped;
4. That internal circulation on the School District Parcel be such as will avoid any parking problems;
5. That the standards set forth in the Sylvan-Dale Area Plan relating to said parcel remain in full force and effect, except as expressly modified herein.

The foregoing Resolution was regularly introduced and adopted by the City Council of the City of Mountain View at a Regular Meeting duly held on the 12th day of November, 1974, by the following vote:

AYES: Councilmembers Allen, Anderson, Gordon, Jelavich, Perez and Mayor Cusimano
NOES: Councilmember Moss
ABSENT: None
NOT VOTING: None

ATTEST:

JEAN HIXSON
CITY CLERK

APPROVED:

JOSEPH CUSIMANO
MAYOR

I do hereby certify that the foregoing resolution was passed and adopted by the City Council of the City of Mountain View at a Regular meeting held on the 12th day of Nov., 1974 by the foregoing vote.

City Clerk
City of Mountain View

CITY OF MOUNTAIN VIEW
RESOLUTION NO. 10365
Series 1975

A RESOLUTION AMENDING THE SYLVAN-DALE AREA PLAN IN ORDER TO ACCOMMODATE REZONING OF LANDS ALONG EVELYN AVENUE TO EITHER THE ML (LIMITED INDUSTRIAL) OR THE P (PLANNED COMMUNITY) DISTRICT

WHEREAS, the City Council has received and considered a report dated January 7, 1975 from the Planning Commission recommending certain amendments to the Sylvan-Dale Area Plan; and

WHEREAS, the City Council has held public hearings on proposed amendments to the Sylvan-Dale Area Plan;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Mountain View that the Sylvan-Dale Area Plan be amended as follows:

Page 34 to read:

"The Plan designates the lands along Evelyn Avenue from Bernardo to Moorpark for light industrial uses. Approximately 9 acres of this industrial area are already developed and approximately 6.8 acres remain for development. Light industrial use, rather than expansion of the general industrial uses, would help insure a better relationship between the industrial lands and abutting residential lands to the south. Attention must be given to physical buffering between the uses.

"In order to insure the proper transition from the general industrial uses to limited industrial uses, and to insure the protection of already developed land, consideration should be given to application of the Planned Community District, where appropriate, with general application of the ML District modified by the following provisions:

1. In addition to the ML uses, the following shall be considered principal permitted uses within the subject area:

   a. Wholesale uses.
   b. Warehousing and indoor storage of non-hazardous materials.
   c. Accessory uses customarily appurtenant to a permitted use.
   d. Properly screened or fenced outside storage amounting to no more than 10% of the covered area.

2. All existing occupancies shall be considered permitted uses. Changes in occupancies that are to occupancies allowed above or that do not increase the intensity of existing use shall be subject to the administrative review of the Zoning Administrator following normal review procedures for the ML District, and shall not require a PC Permit.

3. Existing structures may be reconstructed at MM development standards in the event of major damage.

4. Specific development criteria:

a. A minimum five-foot landscaped buffer-screen zone along the rear property line, unless building wall is located at rear property line, in such case, rear elevation to be treated in an architecturally aesthetic manner.

b. No use permitted that would emit excessive noise or odor pollution or lighting obtrusive to adjacent uses.

5. Signing, outdoor storage, parking changes and minor structural changes shall be subject to administrative approval of the Zoning Administrator. Other changes in land use and new construction and structural additions, not listed above, shall require a PC Permit from the City Council after recommendation by the Zoning Administrator."

Page 67 to read:

"1. LOCATION: Industrial land along Moorpark and Evelyn Avenue.

ACTION: Rezone from MM (General Industrial) District to the ML (Limited Industrial) District or P (Planned Community) District.

DESCRIPTION: This rezoning would involve approximately 16 acres of land consisting of 5 major parcels, two of which are currently undeveloped. One parcel is developed with a light industrial building on a 5.8 acre parcel, the other two are developed with industrial buildings with uses including a flower distribution operation, print shop and auto-oriented services.

EXPLANATION: These industrial lands directly abut residential uses within the cities of Sunnyvale and Mountain View. The nature of the MM Zone would permit heavy industrial uses including outdoor storage and activites which are contrary to the best interest of adjacent residential uses. The ML or P Zone would preserve the light industrial character of these uses and enhance the relationship between the industrial land and adjacent residential land. The application of the P zone, with appropriate review provisions suggested earlier in this Area Plan, would provide for a means of reviewing the transition from existing uses to new uses within the area and avoid possible con-

flicts caused by the nonconforming provisions of the ordinance."

The foregoing Resolution was regularly introduced and adopted at an Adjourned Regular Meeting of the City Council of the City of Mountain View, duly held on the 27th day of January, 1975, by the following vote:

AYES: Councilmembers Allen, Anderson, Moss, Perez and Mayor Cusimano
NOES: None
ABSENT: Councilmembers Gordon and Jelavich
NOT VOTING: None

ATTEST:

JEAN HIXSON
CITY CLERK

APPROVED:

JOSEPH CUSIMANO
MAYOR

I do hereby certify that the foregoing resolution was passed and adopted by the City Council of the City of Mountain View at the Adjourned Regular Meeting held on the 27th day of January 1975 by the following vote.

City Clerk
City of Mountain View

CITY OF MOUNTAIN VIEW
RESOLUTION NO. 11297
SERIES 1976

A RESOLUTION APPROVING RECOMMENDATION OF ENVIRONMENTAL PLANNING COMMISSION FOR PRECISE PLAN AMENDMENT OF THE SYLVAN-DALE AREA PLAN TO ALLOW RESIDENTIAL USES ON P-ZONED PROPERTY

WHEREAS, the City Council has received and considered a report dated December 21, 1976 from the Environmental Planning Commission recommending a Precise Plan amendment of the Sylvan-Dale Area Plan to allow residential uses on P-zoned property;

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Mountain View that the recommendation of the Environmental Planning Commission on the request from Osterlund Enterprises, Inc. for Precise Plan amendment of the Sylvan-Dale Area Plan to allow residential uses on the P-zoned property located between El Camino Real East and Rainbow Drive, and limiting the number of units to 36, and providing for usable open space, is hereby approved.

The foregoing Resolution was regularly introduced and adopted at an Adjourned Regular Meeting of the City Council of the City of Mountain View duly held on the 27th day of December, 1976, by the following vote:

AYES:	Councilmembers Allen, Gordon, Lyon, Moss, Wilmuth and Mayor Perez
NOES:	Councilmember Frosolone
ABSENT:	None
NOT VOTING:	None

ATTEST:

Alice Roylance
ALICE ROYLANCE
CITY CLERK

APPROVED:

Joseph R. Perez
JOSEPH R. PEREZ
MAYOR

I do hereby certify that the foregoing [illegible] adopted by the [illegible] of Mountain View at an adjourned regular meeting [illegible] 27th [illegible] of Dec., 1976, [illegible]

Alice Roylance
City Clerk
City of Mountain View

CITY OF MOUNTAIN VIEW
RESOLUTION NO. 12982
SERIES 1980

A RESOLUTION AMENDING THE SYLVAN-DALE AREA PLAN WITH RESPECT TO LANDS ALONG DALE AVENUE BETWEEN GREENVIEW DRIVE AND HEATHERSTONE WAY (CHERRY CHASE GOLF COURSE SITE), DISPOSAL OF 4.2+ ACRES PREVIOUSLY PLANNED FOR PARK USAGE, AND ESTABLISHMENT OF DEVELOPMENT CRITERIA TO GUIDE DEVELOPMENT OF SAID LANDS

A resolution amending the Sylvan-Dale Area Plan with respect to lands along Dale Avenue between Greenview Drive and Heatherstone Way (Cherry Chase Golf Course Site), disposal of 4.2+ acres previously planned for park usage, and establishment of development criteria to guide development of said lands.

WHEREAS, the City Council has received and considered reports from the Environmental Planning Commission dated May 7, 1979 and January 30, 1980, as well as other reports which examine development options for certain lands covered by the Sylvan-Dale Area Plan; and

WHEREAS, an Environmental Impact Report setting forth environmental considerations has been prepared, certified and duly considered; and

WHEREAS, the Environmental Planning Commission and the City Council have held public hearings on the proposed amendments of the Sylvan-Dale Area Plan;

NOW THEREFORE, BE IT RESOLVED by the City Council of the City of Mountain View that the Sylvan-Dale Area Plan be amended as follows:

A. Pages 24 and 25, figure C and D, as well as all other base maps should be amended to delete the reference to the public mini park at the corner of Dale Avenue and Heatherstone Way, and to add the potential for housing on the Cherry Chase golf course lands and the former park area.

B. Page 30, a section shall be added prior to the final paragraph to read: A unique mixed residential and open space use of overall low density may be developed along Dale Avenue on the site of the Cherry Chase golf course. The development shall include the continued use of an improved nine hole public golf course which shall also serve as a setting for a development of up to 150 residential units. The design of the project shall take into account not only the need for additional housing in the community, but also the need to protect the recreation and open space values of the existing golf course.

The following criteria shall be followed in carrying out any such combination residential/golf course development:

1. A well designed, functional nine hole golf course open to the public shall be retained and integrated with the residential use.

2. The development shall include all of the approximately 30 acres shown on attached figure D-1 (Golf Course Development Concept) and shall include the coordinated use of the recreational clubhouse facilities which lie within the City of Sunnyvale along Knickerbocker Drive.

3. Not more than 150 residential units may be developed on the site.

4. Buildings shall be limited to a height from grade of not more than three stories.

5. Fifty percent of the units must be at least three bedroom and 80 percent of the units must have at least two bedrooms.

6. The development shall include appropriate on-site recreational facilities to serve diverse ages of project residents.

7. The site shall ensure visual access to the golf course from the street. Setbacks, sidewalk and streetscape design along Dale Avenue shall respect the need to foster the sense of public open space. The streetscape shall include special design features including, at least, the use of meandering walks, provision of benches and clustering of trees.

8. Any development applicant shall be fully responsible for providing to the complete satisfaction of the City Council, at the time of consideration of a specific development plan, private means, conditions and assurances for future retention of the golf course, and, in the event the golf course should cease, that title to the golf course area goes to the Homeowner's Association which shall be responsible for maintaining that area as private open space unless a use change is approved as hereinafter provided. Said assurances shall include a provision in the CC&R's running in favor of the Homeowner's Association that a change in use of the golf course from a golf course or open space shall not take place without a favorable vote of at least 80% of the members of the Homeowner's Association eligible to vote, without the exercise of cumulative or weighted voting. Said assurances shall also include a scenic easement in favor of the Homeowner's Association guaranteeing retention of the open space (although incidental accessory structures may occur with prior City approval), unless a favorable vote of at least 80% of the members of the Homeowner's Association eligible to vote is obtained, without the use of cumulative or weighted voting. The wording of this change-in-use provision and the scenic easement shall be subject to approval by the City Attorney. Any excercise by the Homeowners's Association of its right to change said provisions cannot be exercised without an 80% vote in the same manner as above noted, nor may the Homeowner's Association approve any change in said provisions without the prior consent of the City Council.

9. The design concept generally depicted in figure D-1 shall be carried through and implemented with the coordinated input of the highest levels of architecture, landscape architecture and golf course design talent in order to ensure quality, compatible development of the golf course, landscaping and housing.

C. Amend page 35, last two paragraphs and page 36 first and second paragraphs to read as follows:

Of the 340 acres of land within the Sylvan-Dale Area, approximately 40 are designated for public and quasi-public open uses. This acreage includes approximately 25 acres for the golf course in the Dale triangle which may be redeveloped in conjunction with housing. The acreage does not include the extensive amount of private open and recreational areas in the Sylvan-Dale area.

The Sylvan Park located north of El Camino is designed to serve the bulk of the residential area. Implementation of a pedestrian/bicycle overpass across Route 85 will provide improved access from the Dale area to Cuesta Park, Cooper Park, Huff School and the Stevens Creek Park chain.

D. Amend page 38, paragraph 1, by the addition of the following statement: :

The Sylvan-Dale Plan is based upon the continued use of the Cherry Chase golf course as a quasi-public open space facility, even though developed in conjunction with integrated housing.

E. Page 38, paragraph 2, delete the second paragraph.

F. Page 55, delete paragraphs 3 and 4, the sections on Dale Park and cost.

G. Page 66, delete figure G.

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The foregoing Resolution was regularly introduced and adopted at an Adjourned Regular Meeting of the City Council of the City of Mountain View, duly held on the 3rd day of March, 1980, by the following vote:

AYES: Councilmembers Allen, Figueroa, Frosolone, Moss, Perry, and Mayor Nichols

NOES: Councilmember Wilmuth

ABSENT: None

NOT VOTING: None

ATTEST:

ALICE ROYLANCE
CITY CLERK

APPROVED:

LESLIE C. NICHOLS
MAYOR

Y/M3/A27-29

CITY OF MOUNTAIN VIEW
RESOLUTION NO. 13240
SERIES 1980

A RESOLUTION APPROVING A PRECISE PLAN TO GOVERN DEVELOPMENT OF A 10-ACRE PARK AND SCHOOL RESERVE IN CONJUNCTION WITH BETWEEN 70 AND 100 RESIDENTIAL UNITS IN THE SYLVAN-DALE AREA

WHEREAS, Section 36.22.5.1 of Article III of Chapter 36 of the Mountain View City Code sets forth a procedure whereby the City may adopt a Precise Plan, pertaining to designated real properties, to delineate uses, relationships to other areas, intensity of use, circulation, design criteria, procedures for development review and special conditions, as to such properties; and

WHEREAS, the said Section 36.22.5.1 requires that both the City's Environmental Planning Commission and City Council hold duly noticed public hearings before any Precise Plan is adopted; and

WHEREAS, on September 8, 1980, the Environmental Planning Commission held a duly noticed public hearing and thereafter forwarded its recommendation to the City Council that a Precise Plan be adopted for a 10-acre park and school reserve in conjunction with between 70 and 100 residential units in the Sylvan-Dale area; and

WHEREAS, on September 29, 1980, having given notice as required by City Code Section 36.22.5.1(c), the City Council held a public hearing to consider adoption of said Precise Plan.

NOW, THEREFORE, BE IT RESOLVED AS FOLLOWS:

1. That certain document, consisting of three pages, entitled Precise Plan-Moore Site, attached hereto and incorporated herein by reference as Exhibit "A," is hereby adopted as the Precise Plan for a 10-acre park and school reserve in conjunction with betweeen 70 and 100 residential units in the Sylvan-Dale area.

The foregoing Resolution was regularly introduced and adopted at an Adjourned Regular Meeting of the City Council of the City of Mountain View, duly held on the 29th day of September, 1980, by the following vote:

AYES: Councilmembers Figueroa, Frosolone, Nichols, Packard, Wilmuth and Mayor Allen

NOES: Councilmember Perry

ABSENT: None

NOT VOTING: None

ATTEST:

ALICE ROYLANCE
CITY CLERK

APPROVED:

MATTHEW A. ALLEN
MAYOR

MK/x
17/B15

CITY OF MOUNTAIN VIEW
RESOLUTION NO. 13258
SERIES 1980

A RESOLUTION APPROVING AN AMENDMENT TO THE SYLVAN-DALE AREA PLAN

WHEREAS, Section 36.22.5.1 of Article III of Chapter 36 of the Mountain View City Code sets forth a procedure whereby the City may adopt or amend an Area Plan, pertaining to designated real properties, to delineate uses, relationships to other areas, intensity of use, circulation, design criteria, procedures for development review and special conditions, as to such properties; and

WHEREAS, the said Section 36.22.5.1 requires that both the City's Environmental Planning Commission and City Council hold duly noticed public hearings before any Area Plan Amendment is adopted; and

WHEREAS, on October 15, 1980, the Environmental Planning Commission held a duly noticed public hearing and thereafter forwarded its recommendations to the City Council that an amendment to an Area Plan be adopted for the Sylvan-Dale area; and

WHEREAS, on November 10, 1980, having given notice as required by City Code Section 36.22.5.1(c), the City Council held a public hearing to consider adoption of said amendment to Area Plan.

NOW, THEREFORE, BE IT RESOLVED by the City Council of the City of Mountain View that:

Sylvan-Dale Area Plan amended and adopted by Resolution No. 12982 is hereby amended by increasing the number of allowable units to be constructed on said area from 150 to 170.

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The foregoing Resolution was regularly introduced and adopted at a Regular Meeting of the City Council of the City of Mountain View, duly held on the 10th day of November, 1980, by the following vote:

AYES: Councilmembers Figueroa, Frosolone, Nichols, Packard, Wilmuth, and Mayor Allen

NOES: Councilmember Perry

ABSENT: None

NOT VOTING: None

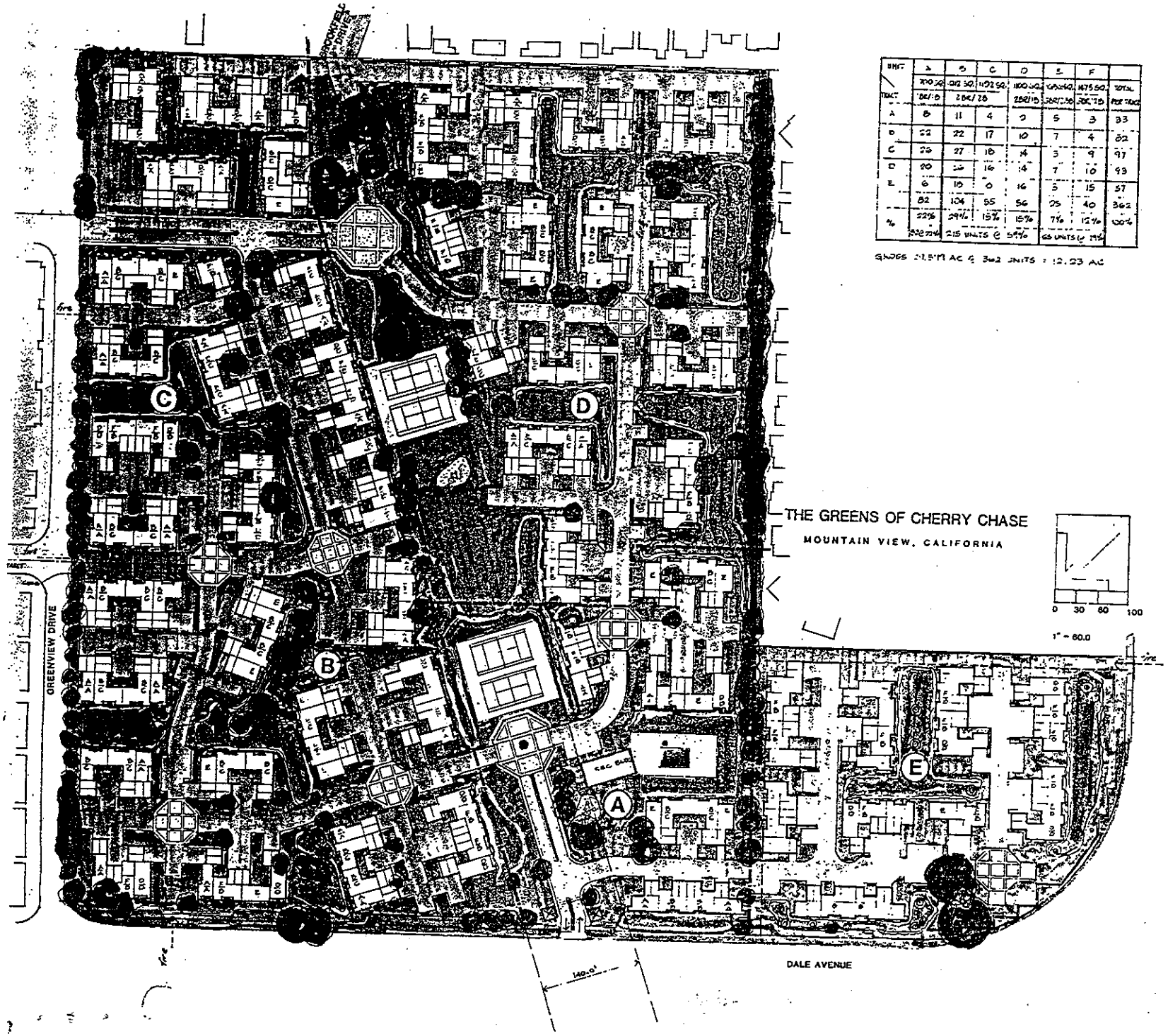
ATTEST:

ALICE ROYLANCE
CITY CLERK

APPROVED:

MATTHEW A. ALLEN
MAYOR

Y17-B4



UNIT	A	B	C	D	E	F	
	700 SQ.	992 SQ.	1192 SQ.	1100 SQ.	1250 SQ.	1475 SQ.	TOTAL
TOTAL	1BR/1B	2BR/2B		2BR/2B	3BR/2B	3BR/2B	PER TOTAL
A	8	11	4	2	5	3	33
B	22	22	17	10	7	4	82
C	26	27	16	14	3	9	97
D	20	26	16	14	7	10	93
E	6	10	6	16	5	15	57
	82	104	55	56	25	40	362
%	22%	29%	15%	15%	7%	12%	100%
	22%	215 UNITS @ 59%			65 UNITS @ 18%		

GROSS 21.5 AC @ 362 UNITS = 12.23 AC

DIVIDEND DEVELOPMENT CORPORATION

DEVELOPER

Gentry/Briggs Asso
Architecture and Pla
Don L. Gentry and Terry L. Briggs
3000 Pulvenage Avenue
Santa Clara, CA 95051 (408)

CHERRY CHASE II
SCHEME I

8206

CITY OF MOUNTAIN VIEW
ORDINANCE NO. 04-05
SERIES 2005

AN ORDINANCE AMENDING THE ZONING MAP FOR
505 EAST EVELYN AVENUE FROM SYLVAN-DALE PRECISE PLAN TO
R3-2.2 (MULTIPLE-FAMILY RESIDENTIAL)

WHEREAS, Chapter 36 of the Mountain View City Code sets forth a procedure whereby the City may amend the City's Zoning Map; and

WHEREAS, said Chapter 36 of the Mountain View City Code requires that both the City's Environmental Planning Commission and City Council hold a duly noticed public hearing before the City's Zoning Map is amended; and

WHEREAS, on March 16, 2005, the Environmental Planning Commission held a duly noticed public hearing and thereafter forwarded its recommendation to the City Council that the City's Zoning Map be amended to change the designation of three parcels located at 505 East Evelyn Avenue from the Sylvan-Dale Precise Plan to the R3-2.2 (Multiple-Family Residential) Zone; and

WHEREAS, on April 12, 2005, having given notice as required by Chapter 36 of the Mountain View City Code, the City Council held a public hearing to consider the rezoning of said parcels and the amendment of the City's Zoning Map;

NOW, THEREFORE, BE IT RESOLVED AS FOLLOWS:

1. The zoning designation for the three parcels located at 505 East Evelyn Avenue is hereby changed from the Sylvan-Dale Precise Plan to the R3-2.2 (Multiple-Family Residential) Zone, and the Zoning Map is hereby amended to reflect such change, all as more particularly described on Exhibit "A," attached hereto and incorporated herein.

The foregoing ordinance was regularly introduced at the Special Meeting of the City Council of the City of Mountain View, duly held on the 12th day of April, 2005, and thereafter adopted at the Special Meeting of said Council, duly held on the 26th day of April, 2005, by the following roll call vote:

AYES:	Councilmembers Galiotto, Kasperzak, Means, Pear, Perry and Mayor Neely
NOES:	Councilmember Macias
ABSENT:	None
NOT VOTING:	None

ATTEST:

ANGELITA M. SALVADOR
CITY CLERK

APPROVED:

MATT NEELY
MAYOR

I do hereby certify that the foregoing ordinance was passed and adopted by the City Council of the City of Mountain View at a Special Meeting held on the 26th day of April, 2005, by the foregoing vote, and was published in the *San Jose Post Record* by reference on the 22nd day of April, 2005, and posted in three prominent places in said City.

City Clerk
City of Mountain View

MA/8/ORD
891-04-12-05o^

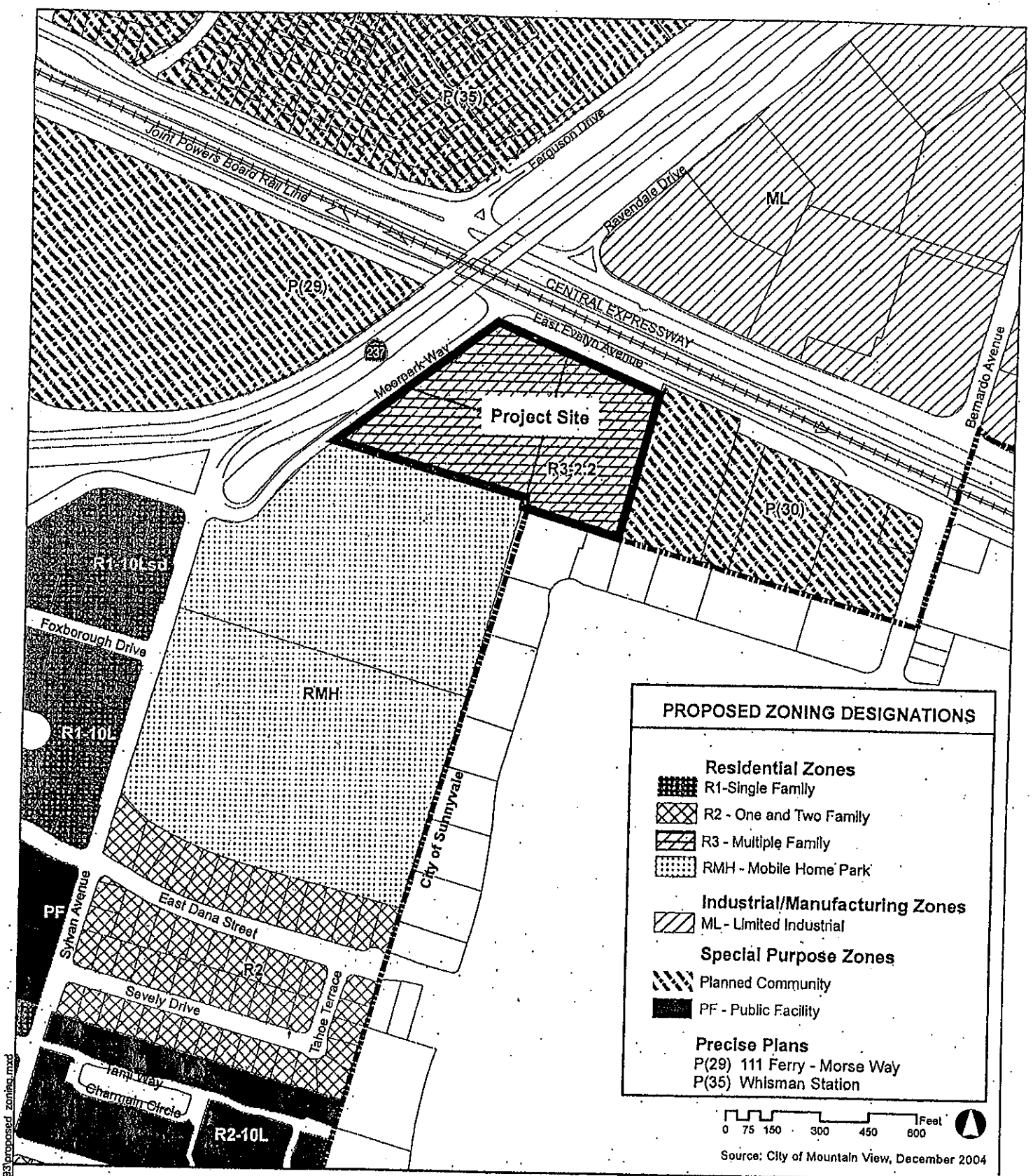


FIGURE 3
Proposed Zoning, 505 East Evelyn Avenue

EIP
10993-00

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505 East Evelyn Avenue Project